

MEMORANDUM

To: Chair and Members and District 7 and 8 Planning Advisory Committee

From: Jillian MacLellan, Planner, HRM Development Approvals

Date: January 27, 2014

Subject: **Case 18270: Application by Genivar for the lands of 5885 Spring Garden Road to develop a mixed use building through a development agreement**

An application has been received from Genivar to develop a 20 storey residential addition to an existing building at 5885 Spring Garden Road. The proposed development also includes a single storey commercial addition to the front of the existing building and improvements to the amenity areas and landscaping around the site.

A copy of the application submission, including the proposed development plans, wind impact assessment, shadow analysis and traffic impact statement are attached to this memo (see Attachments A to G).

Existing Use

The site is currently developed as an 11 storey multiple unit residential building with a dental office at the ground floor. There is a combination of underground and surface parking. The proposed addition is to be located to the rear of the property which is currently used for at grade parking.

There is an existing development agreement on the property which allows for the consideration of a dental office at the ground level of the building. The development agreement requires that 6 parking spaces be provided for the dental office use.

Location and Planning Context

The property is 65,340 square feet and is located at the north-east corner of Spring Garden Road and Carlton Street. There is a mix of low to high density residential uses located in the area and a mix of office and retail commercial uses.

The property is bordered by Camp Hill Cemetery and is a block away from the Public Gardens. There are various municipally registered heritage buildings in the area, including the Garden Crest Apartment, which is also a provincially registered

heritage property and directly abuts the subject property (see Maps 1 and 2).

The subject property is designated High Density Residential in the Peninsula Centre Secondary Plan of the Municipal Planning Strategy for Halifax (MPS) and is within the Spring Garden Road Sub Area.

The property zoned R-3 (Multiple Dwelling Zone) in the Land Use By-law for Halifax Peninsula (LUB) and is within a height precinct of 45 feet. Further, the property is located within Schedules A and B of the Land Use By-law. Schedule A allows for developments with a higher density than what is permitted throughout the majority of the Halifax Peninsula area. Schedule B allows commercial uses in a residential zone which are associated with the residential use. For example a convenience store may be permitted in a multiple unit dwelling, but should only be accessed through the interior of the building and should have no external signage (see Attachment H).

Applicable Policy Policy 8.1.2 of the Peninsula Centre Secondary Plan of the MPS allows for the consideration of buildings that would exceed the height precinct through a development agreement. The policy requires that the shadow impact on the public gardens, during the period of the year that the public gardens is open, be considered.

Policy 3.11 allows for the further consideration of varying other aspects of the LUB for developments considered under Policy 8.1.2 as long as the intent of the policies of the MPS and Secondary Plan is maintained. (See Attachment I).

Proposal The proposed 20-storey residential building addition would be attached to rear of an existing 11-storey mixed residential and commercial building and includes 110 residential units in addition to the existing 201 units in the existing building. The proposed addition is approximately 211 feet in height and consists of a tower with approximately 8.5 metre separation between proposed tower and existing building. Other features of the proposed development include the following:

- Commercial uses located at the ground level of the existing building along Spring Garden Road.
- Commercial uses or amenity space to be located at the ground level of the proposed addition along Carlton Street.
- Continued vehicular access off of Carlton Street.
- Modifications and expansions to the existing rooftop amenity space.
- Modifications to the existing building including recladding and enhanced landscaping along Spring Garden Road.
- An overall density of approximately 400 persons per acre (ppa). (Density is calculated by assigning 1 person per bachelor unit, 2 persons per 1 bedroom unit and 2.25 persons per 2+bedroom units).

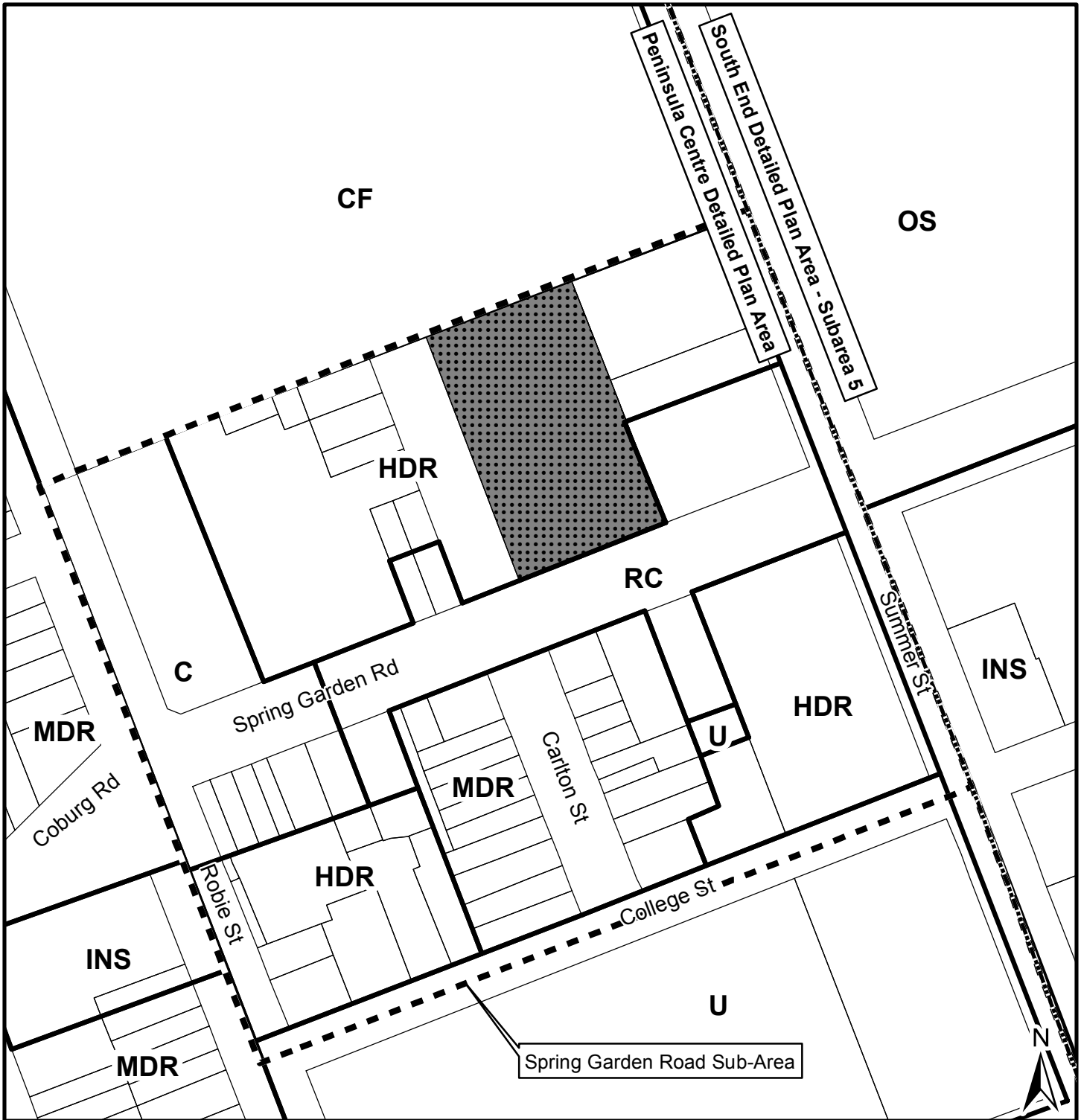
Public Information Meeting A public information meeting was held on September 11, 2013. Approximately 110 people were in attendance. Attachment J contains a copy of the minutes from the meeting.

Input Sought from District 7 & 8 Planning Advisory Committee

Pursuant to the Committee's Terms of Reference, feedback is sought from the Committee relative to the applicable policies of the Municipal Planning Strategy for Halifax. Some specific items that the Committee may wish to address include the proposed height, density and design of the proposed addition.



Attachments

Map 1	Generalized Future Land Use Map (GFLUM) Map
Map 2	Zoning Map
Map 3	Height Precinct
Map 4	Heritage Properties
Attachment A	Rendering
Attachment B	Site Plan
Attachment C	Floor Plans and Elevations
Attachment D	Preliminary Landscape Plan
Attachment E	Shadow Impact Assessment
Attachment F	Wind Impact Study
Attachment G	Traffic Impact Statement
Attachment H	Excerpt from the Halifax Peninsula Land Use By-law
Attachment I	Applicable Policies of Halifax Municipal Planning Strategy
Attachment J	Public Information Meeting Minutes – September 11, 2013



Map 1 - Generalized Future Land Use

5885 Spring Garden Road
Halifax

-  Area of proposed development agreement
-  Detailed plan area boundary

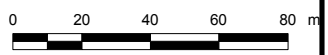
Halifax Plan Area
Peninsula Centre Detailed Plan Area
South End Detailed Plan Area

Designation - Peninsula Centre

- MDR Medium Density Residential
- HDR High Density Residential
- RC Residential - Commercial Mix
- C Commercial
- U University

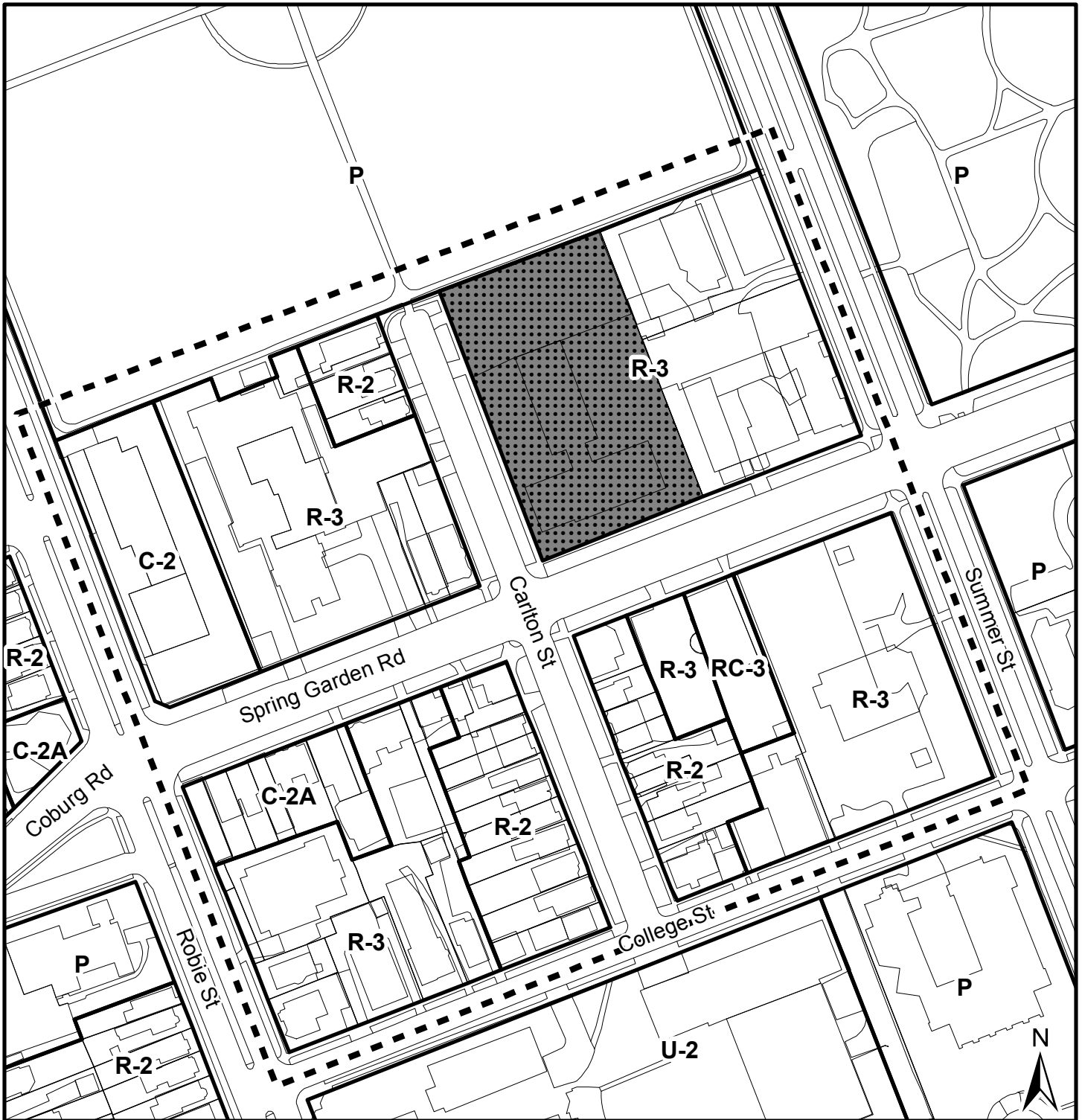
Designation - South End

- OS Open Space
- INS Institutional




This map is an unofficial reproduction of a portion of the Zoning Map for the plan area indicated.


HRM does not guarantee the accuracy of any representation on this plan.



Map 2 - Zoning

5885 Spring Garden Road
Halifax

 Area of proposed development agreement

 Area of notification

Halifax Peninsula
Land Use By-Law Area

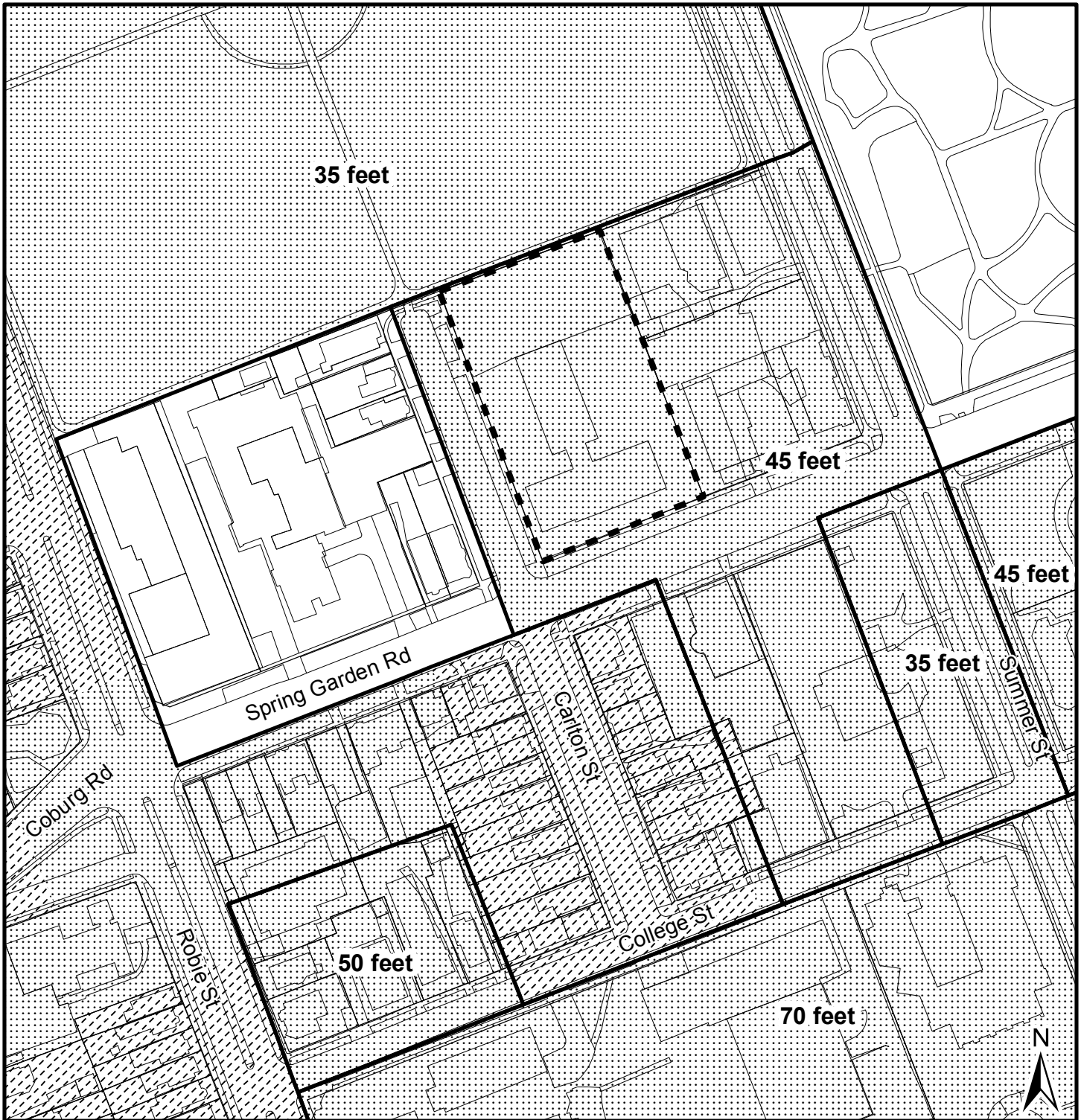
Zone

- R-2 General Residential
- R-3 Multiple Dwelling
- RC-3 High Density Res. Minor Commercial
- C-2 General Business
- C-2A Minor Commercial
- P Park and Institutional
- U-2 High-Density University




This map is an unofficial reproduction of a portion of the Zoning Map for the plan area indicated.

HRM does not guarantee the accuracy of any representation on this plan.



Map 3 - Height Precinct

5885 Spring Garden Road
Halifax

 Area of proposed development agreement



Height measured to highest point of roof



Height measured to highest point of top floor of building (not including non-habitable roof)



Height measured to commencement of top storey of building

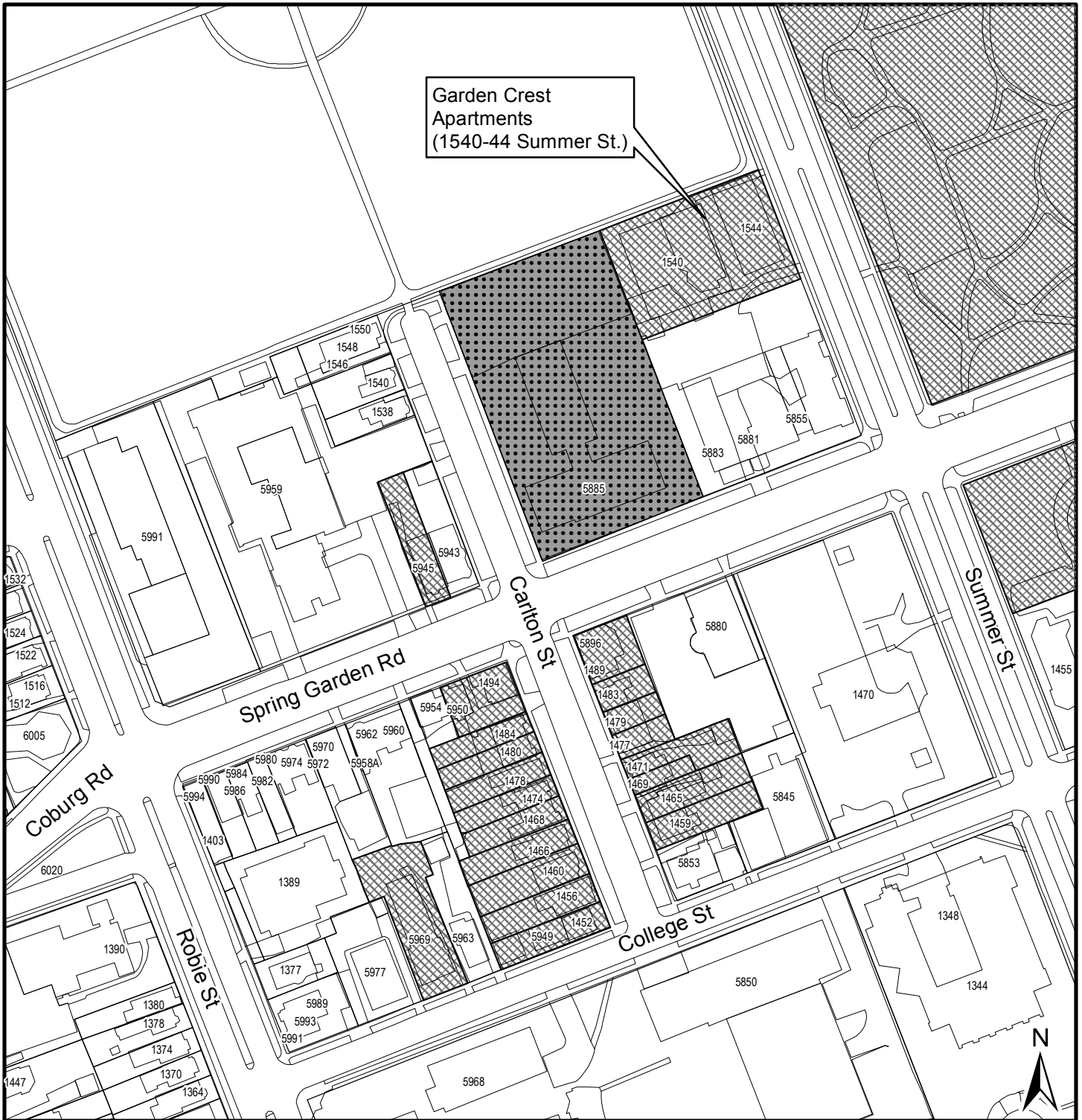
Halifax Peninsula
Land Use By-Law Area

Please refer to ZM-17 - Height Precinct Map for full definitions of height measurement.



This map is an unofficial reproduction of a portion of the Height Precinct Map for the plan area indicated.



HRM does not guarantee the accuracy of any representation on this plan.



Garden Crest
Apartments
(1540-44 Summer St.)

Map 4 - Heritage Properties

5885 Spring Garden Road
Halifax

-  Area of proposed development agreement
-  Municipally registered heritage property

Halifax Peninsula
Land Use By-Law Area



HRM does not guarantee the accuracy of any representation on this plan.



PERSPECTIVE SKETCH -
VIEW LOOKING NORTH FROM CORNER OF SPRING GARDEN AND CARLTON

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC

MICHAEL
APIER
ARCHITECTURE INC

DRAWING

PERSPECTIVE SKETCH

SCALE
AS NOTED

DATE

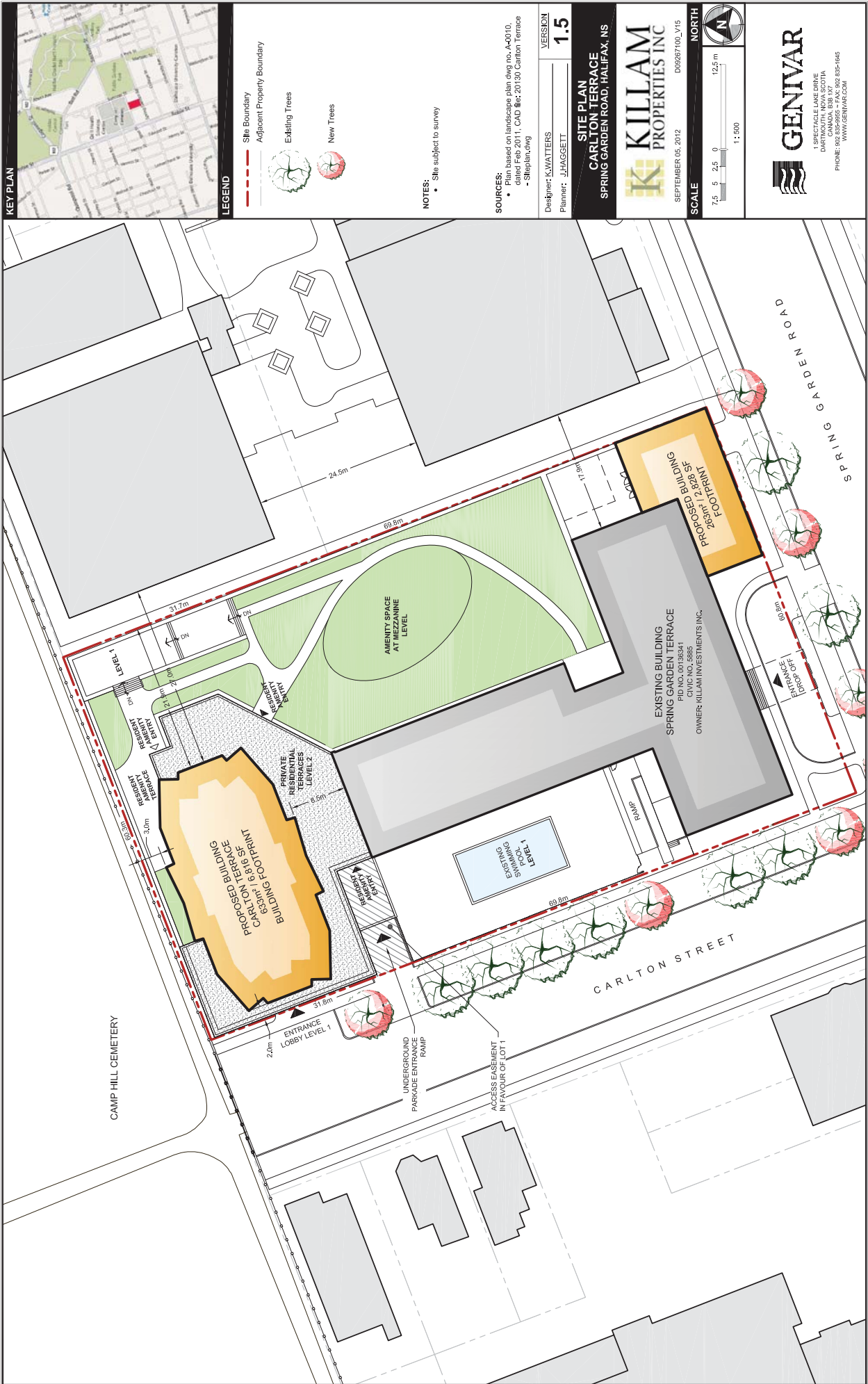
NOV. / 2012

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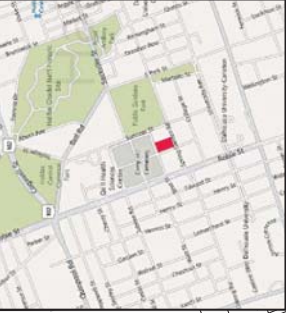
A-914

Attachment B - Site Plan

FILE: K:\GARTNORTH\2009\0928\287100_V15.DWG



KEY PLAN



LEGEND

- Site Boundary
- - - Adjacent Property Boundary
- Existing Trees
- New Trees

NOTES:

- Site subject to survey

SOURCES:

- Plan based on landscape plan dwg no. A-0010, dated Feb 2011, CAD file: 20130 Carlton Terrace - Siteplan.dwg

Designer: KWATTERS
Planner: JHAGGETT
VERSION: **1.5**

SITE PLAN
CARLTON TERRACE
SPRING GARDEN ROAD, HALIFAX, NS



SCALE
7.5 5 2.5 0 12.5 m
1:500

NORTH

GENIVAR
1 SPECTACLE LAKE DRIVE
DARTMOUTH, CANADA, B3B 1X7A
PHONE: 902-855-9555 • FAX: 902-855-1865
WWW.GENIVAR.COM

SITE INFORMATION

SITE AREA: 66,210 sq. ft.
EXISTING BUILDING: 15,834 sq. ft.
PROPOSED BUILDING: 6,816 sq. ft.
BUILT AREA: 22,650 sq. ft.
LOT COVERAGE: 34%
GROSS SITE AREA: 83,100 sq. ft.
 - 1.91 ACRES @ 250 PERSONS/ACRE
 - ALLOWANCE OF 478 PERSONS

EXISTING: 201 UNITS
PROPOSED: 110 UNITS
TOTAL: 311 UNITS X 2.25 PERSONS/UNIT
 = 700 PERSONS (1.46 x DENSITY)

EXISTING PARKING:

- UNDERGROUND 97 CARS
 - SURFACE 60 CARS
 - TOTAL 157 CARS
 - CURRENT TENANT USE 136 CARS (87%)

PROPOSED PARKING:

- EXISTING UNDERGROUND 97 CARS
 - ADDITIONAL UNDERGROUND 121 CARS
 - TOTAL 218 CARS

UNIT TYPES:

TYP. FLOOR PLAN (17 TYP. FLOORS):
 - TWO BEDROOM: 4
 - ONE BEDROOM + DEN: 2

PENTHOUSE (2 FLOORS):
 - TWO BEDROOM + DEN: 4

TOTAL UNITS: 110
 - ONE BEDROOM + DEN: 34
 - TWO BEDROOM: 68
 - TWO BEDROOM + DEN: 8

PROPOSED NEW RESIDENTIAL

AMENITY SPACE:

- LEVEL 1 - 3645 SF
 - LEVEL MEZZANINE - 8155 SF
 - TOTAL AMENITY = 11800 SF

COMMERCIAL SPACE (SPRING GARDEN

TERRACE):

- LEVEL 1 +/- 3600 SF

CARLTON TERRACE

PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



KILLAM
 PROPERTIES INC

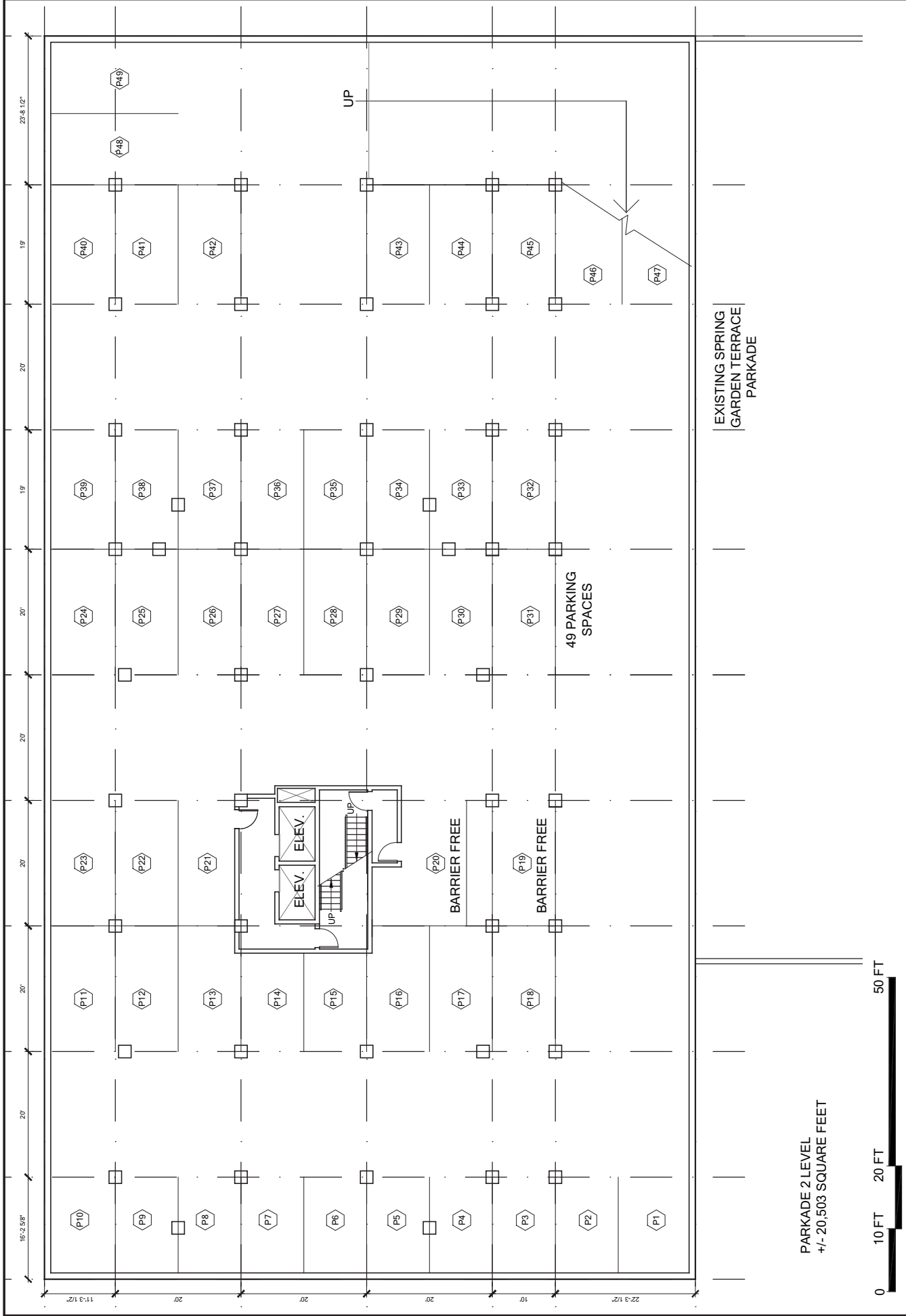


DRAWING PROJECT DATA SHEET

SCALE AS NOTED

DATE OCT 19 2012

DRAWING NO. A-100



PARKADE 2 LEVEL
 +/- 20,503 SQUARE FEET



CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



KILLAM
 PROPERTIES INC.

MICHAEL
 APIER
 ARCHITECTURE INC.

DRAWING
 PARKADE PLAN
 LEVEL 2
 SCALE AS NOTED

DATE
 OCT 19 2012
 DRAWING NO.
A-101



PARKADE 1 LEVEL
 +/- 20,503 SQUARE FEET



CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



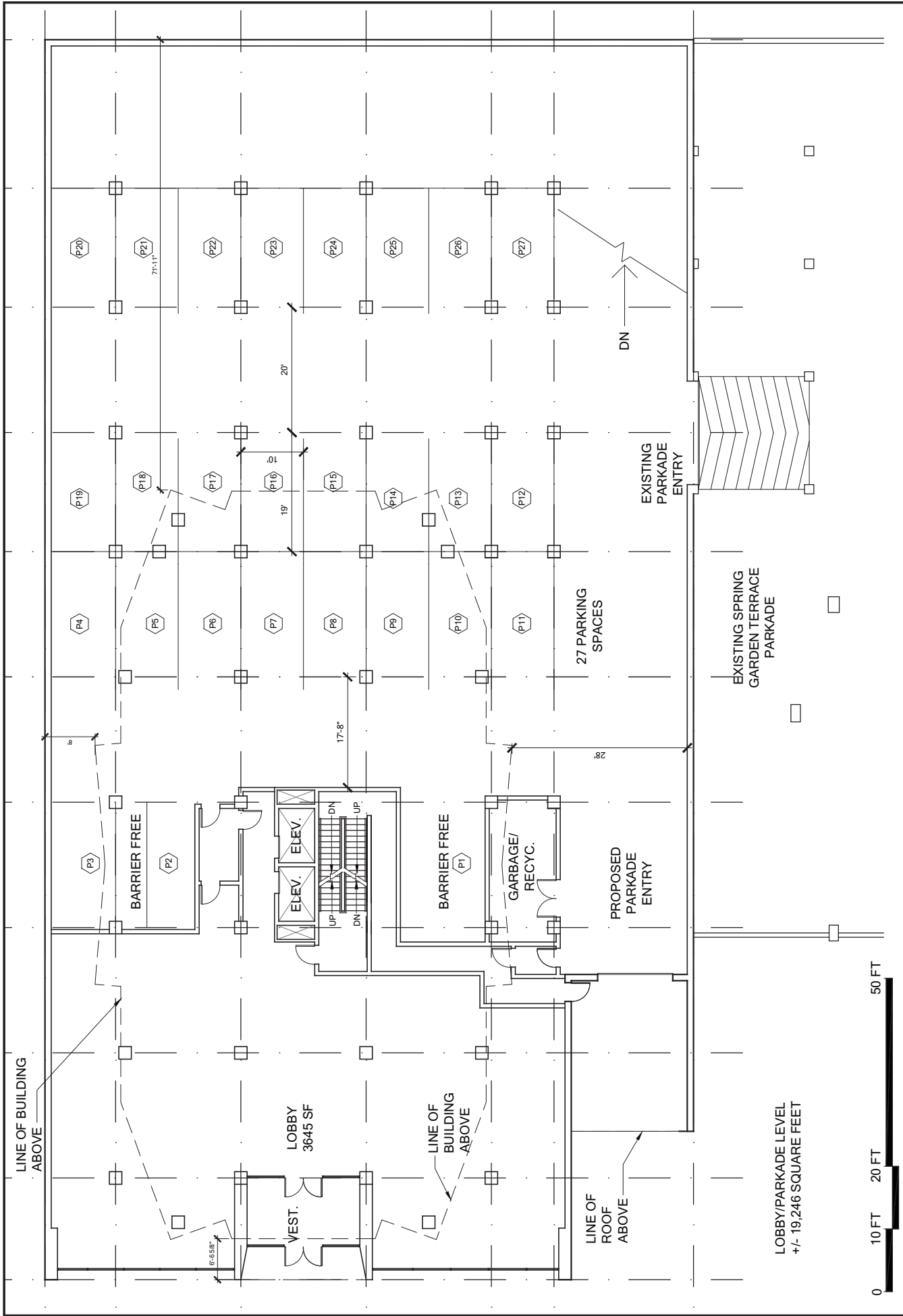
KILLAM
 PROPERTIES INC.



DRAWING
 PARKADE PLAN
 LEVEL 1
 SCALE

AS NOTED

DATE
 OCT 19 2012
 DRAWING NO.
A-102



CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA

KILLAM PROPERTIES INC

MZA ARCHITECTURE INC
 MICHAEL APIER ARCHITECTURE INC

DRAWING: LOBBY / PARKADE LEVEL 1
 SCALE: AS NOTED

DATE: OCT 19 2012
 DRAWING NO.: A-103

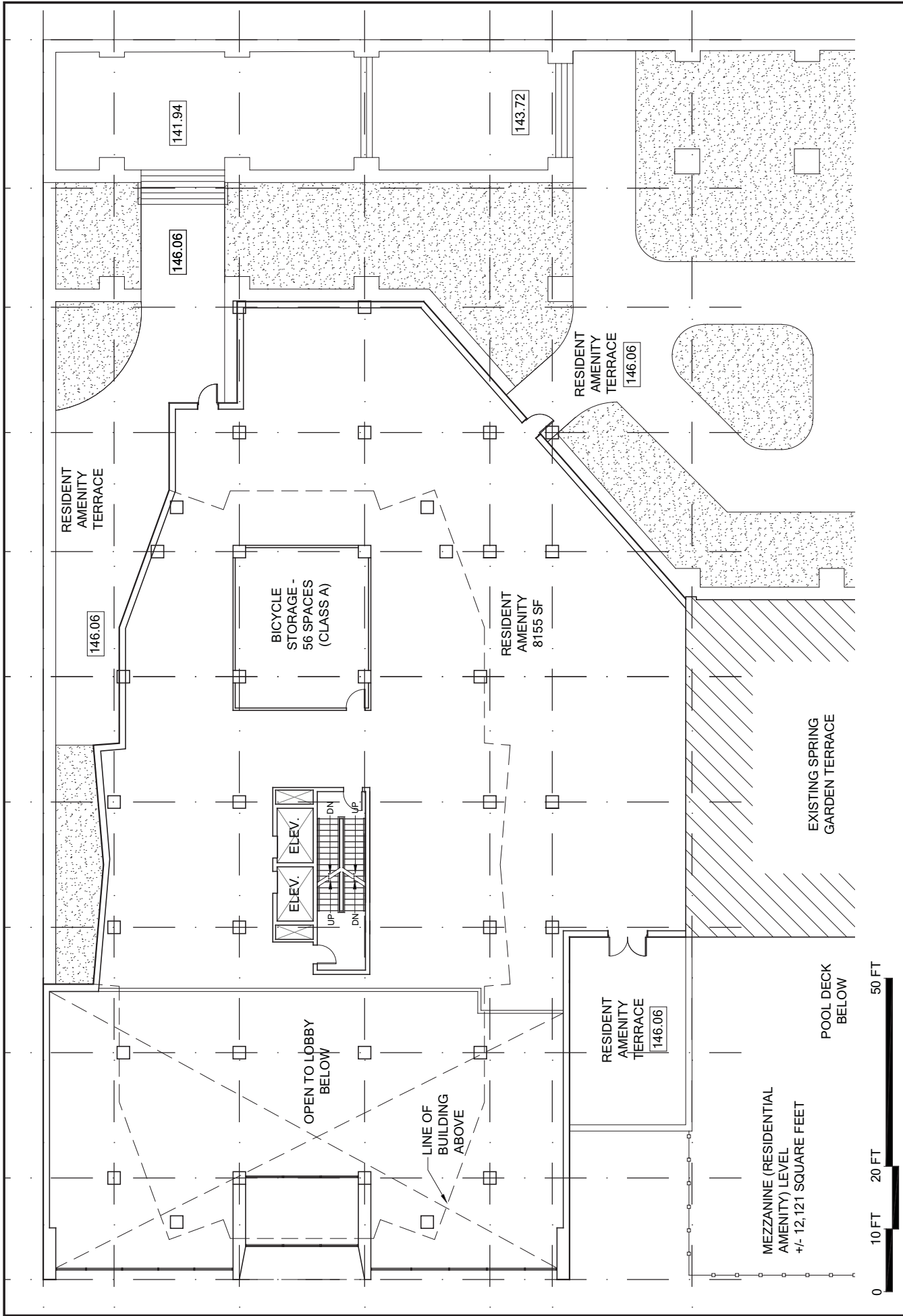


LOBBY/PARKADE LEVEL
 +/- 19,246 SQUARE FEET

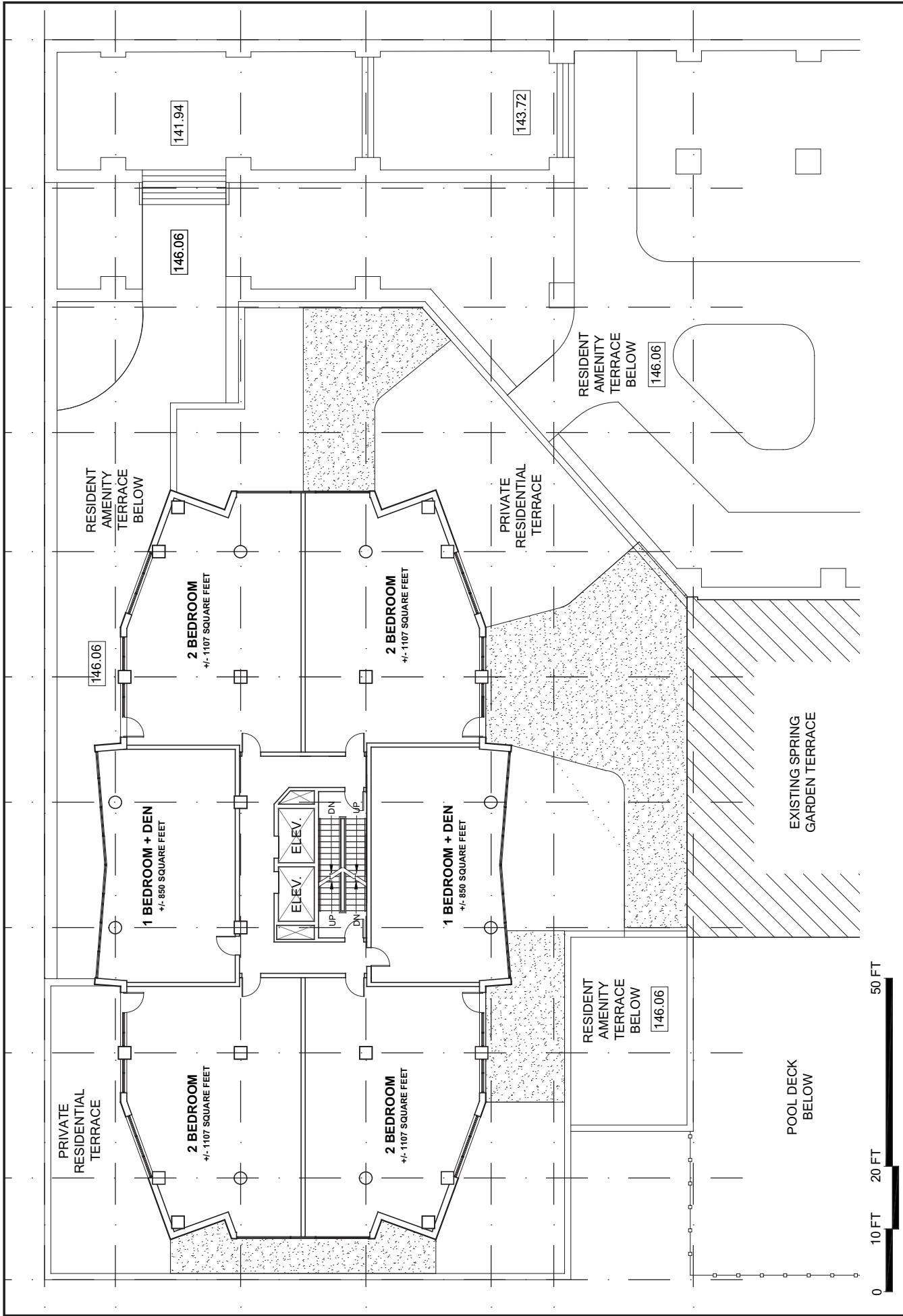
LINE OF ROOF ABOVE

LINE OF BUILDING ABOVE

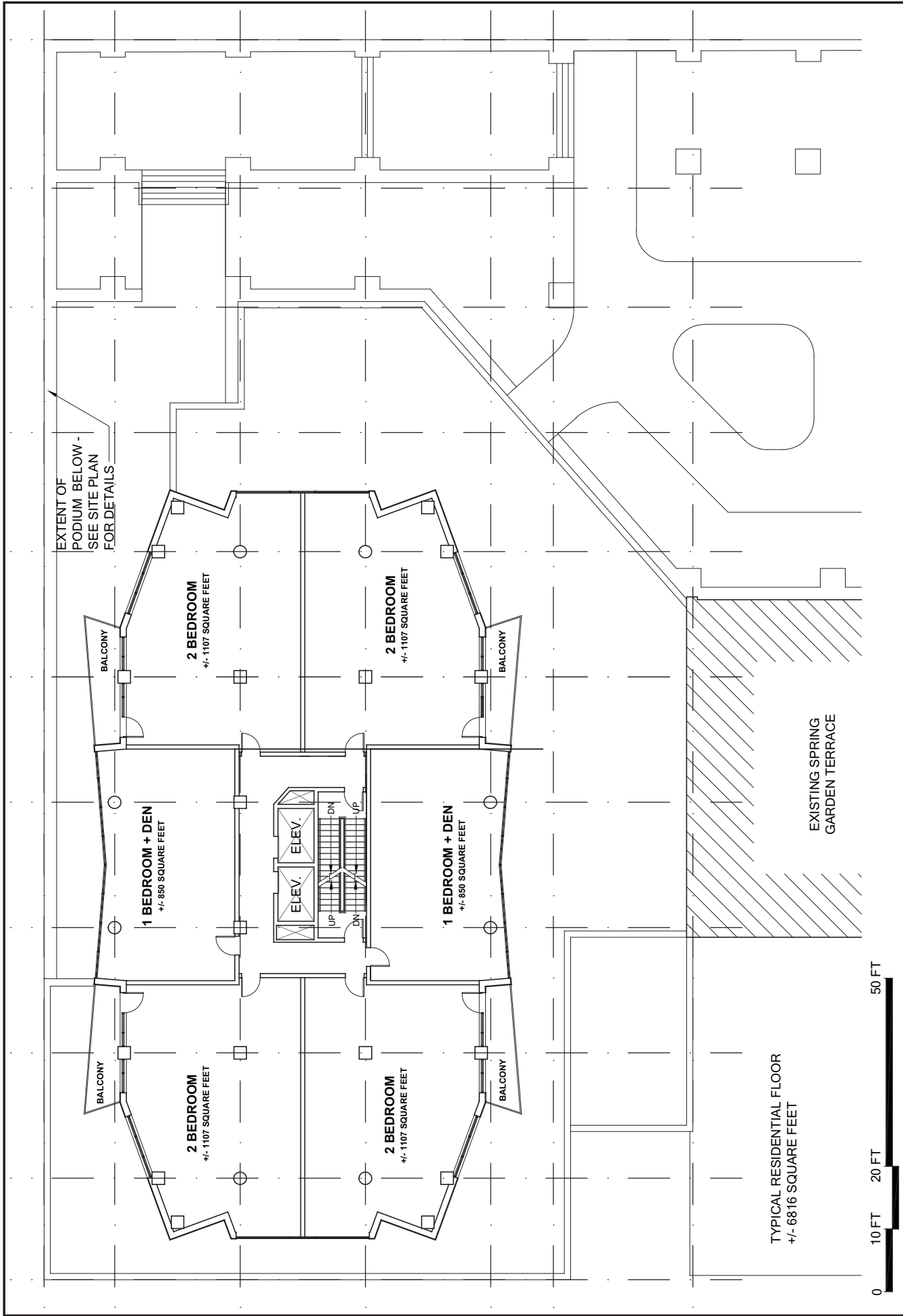
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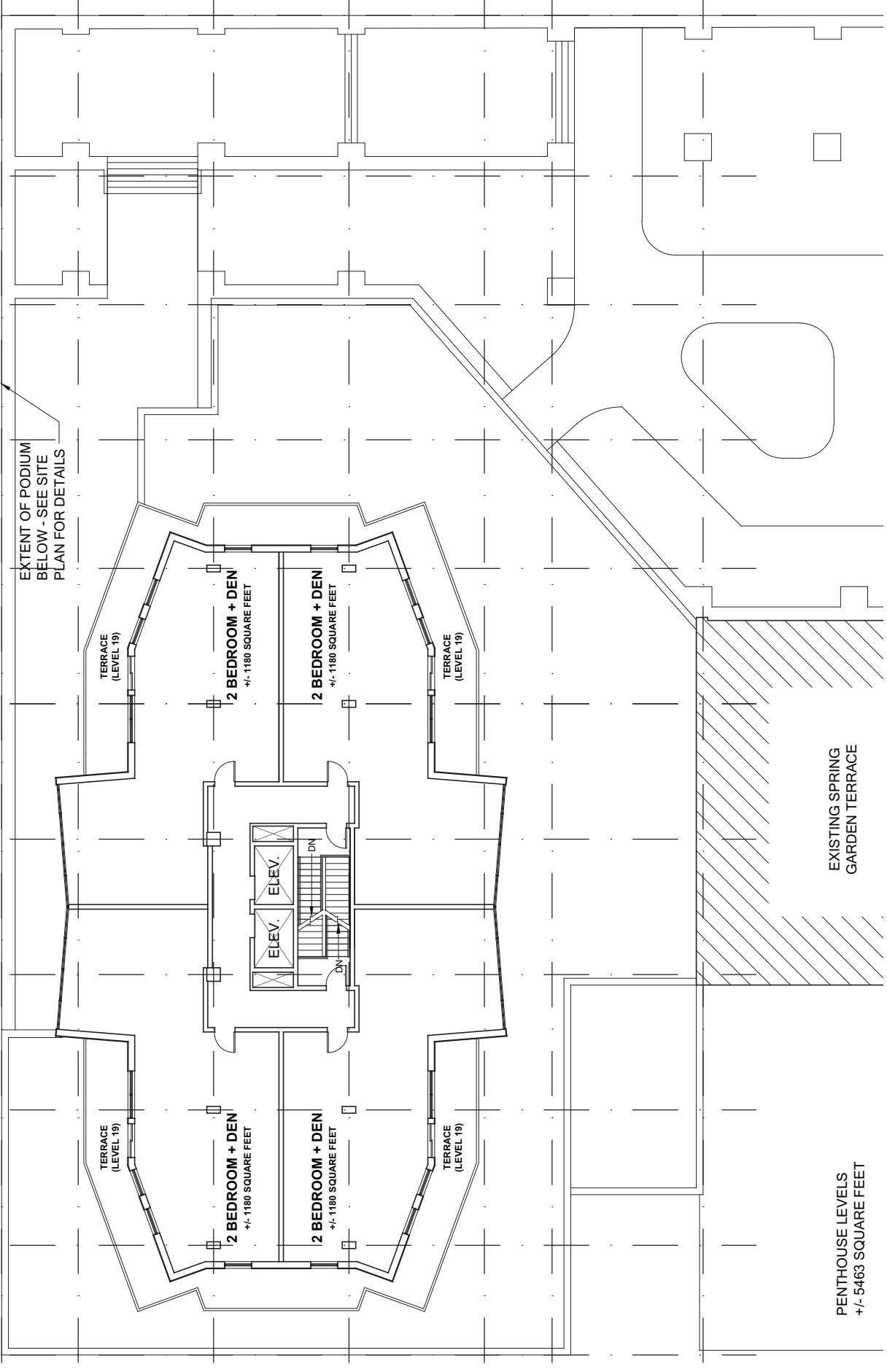
CARLTON TERRACE PROPOSED DEVELOPMENT HALIFAX, NOVA SCOTIA	 KILLAM PROPERTIES INC.	 MICHAEL APIER ARCHITECTURE INC.	DRAWING RESIDENTIAL AMENITY - MEZZANINE LEVEL SCALE	DATE OCT 19 2012
			AS NOTED	DRAWING NO. A-104



DRAWING RESIDENTIAL - LEVEL 2 SCALE	DATE	OCT 19 2012
	DRAWING NO.	A-105
AS NOTED	MICHAEL APIER ARCHITECTURE INC	
CARLTON TERRACE PROPOSED DEVELOPMENT HALIFAX, NOVA SCOTIA		



CARLTON TERRACE PROPOSED DEVELOPMENT HALIFAX, NOVA SCOTIA			DRAWING TYPICAL RESIDENTIAL - LEVELS 3 - 18 SCALE	DATE OCT 19 2012
			AS NOTED	DRAWING NO. A-106



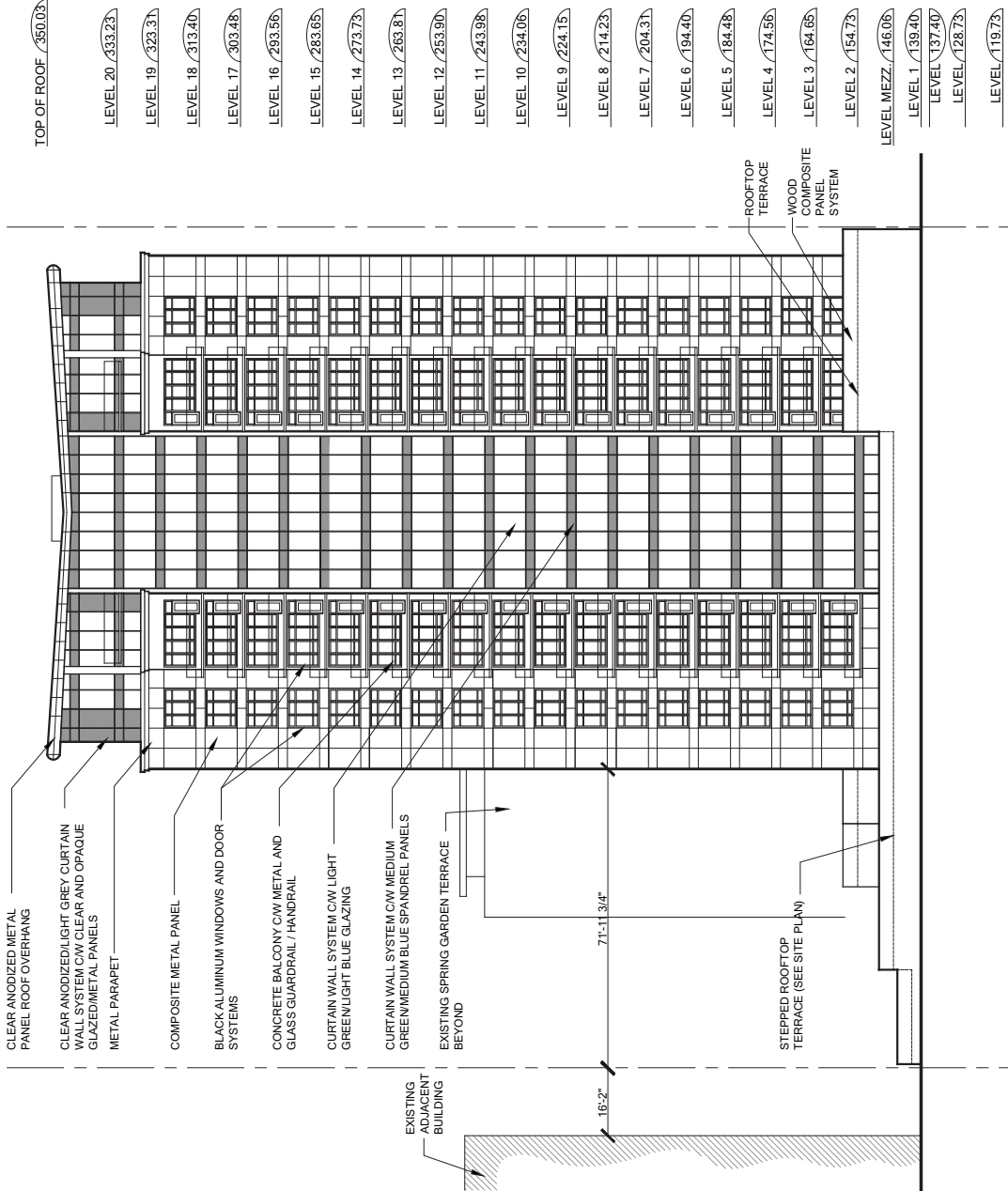
CARLTON TERRACE
PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA

DRAWING
TYPICAL PENTHOUSE
LEVELS 19 - 20
SCALE

AS NOTED

DATE
OCT 19 2012

DRAWING NO.
A-107



0 10 FT 20 FT 50 FT

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC

MICHAEL
APIER
ARCHITECTURE INC

DRAWING
NORTH ELEVATION

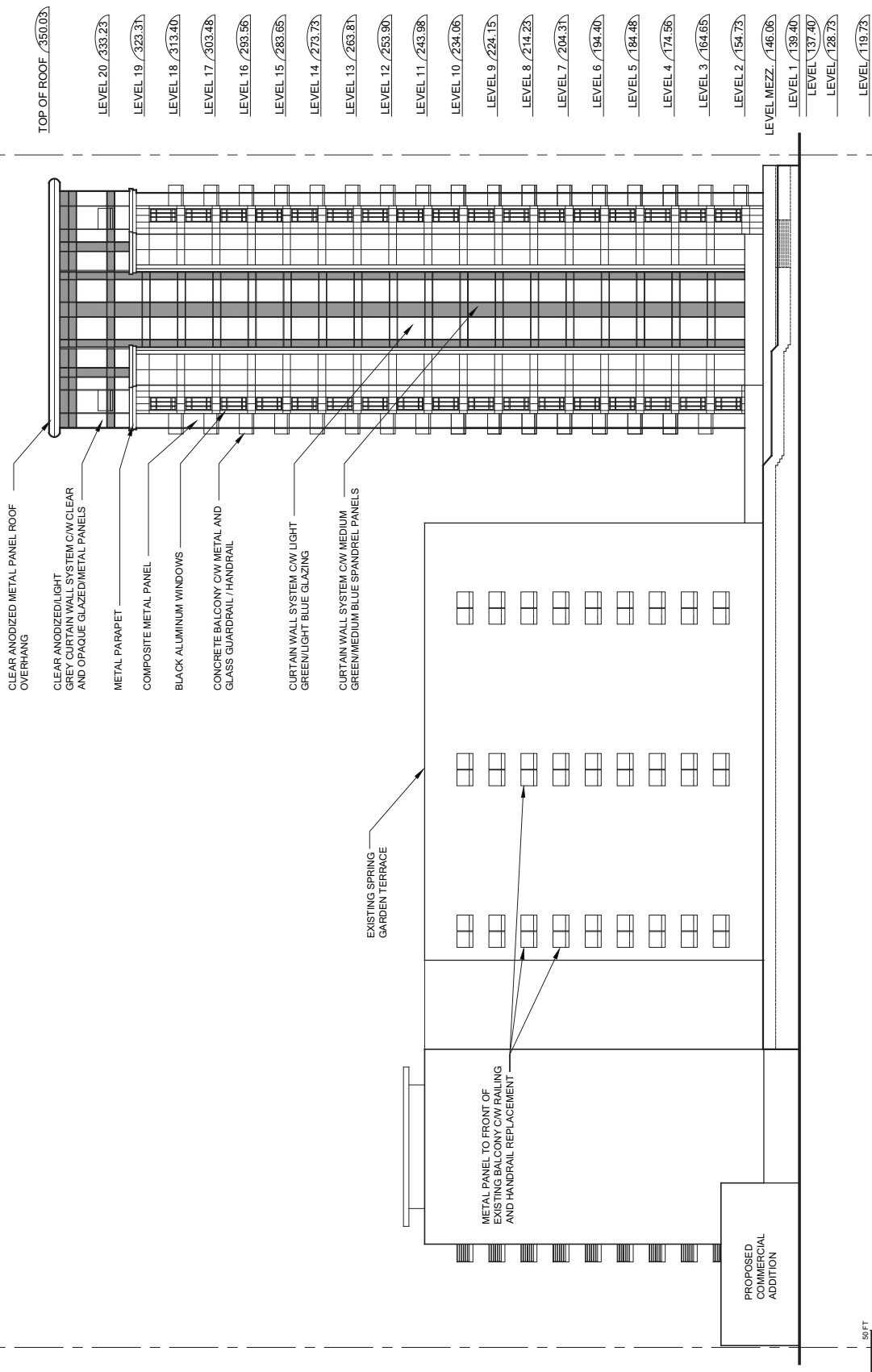
DATE
OCT 19 2012

SCALE
AS NOTED

DRAWING NO.
A-201

PROPERTY LINE

PROPERTY LINE



0 10 FT 20 FT 30 FT

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



DRAWING
EAST ELEVATION

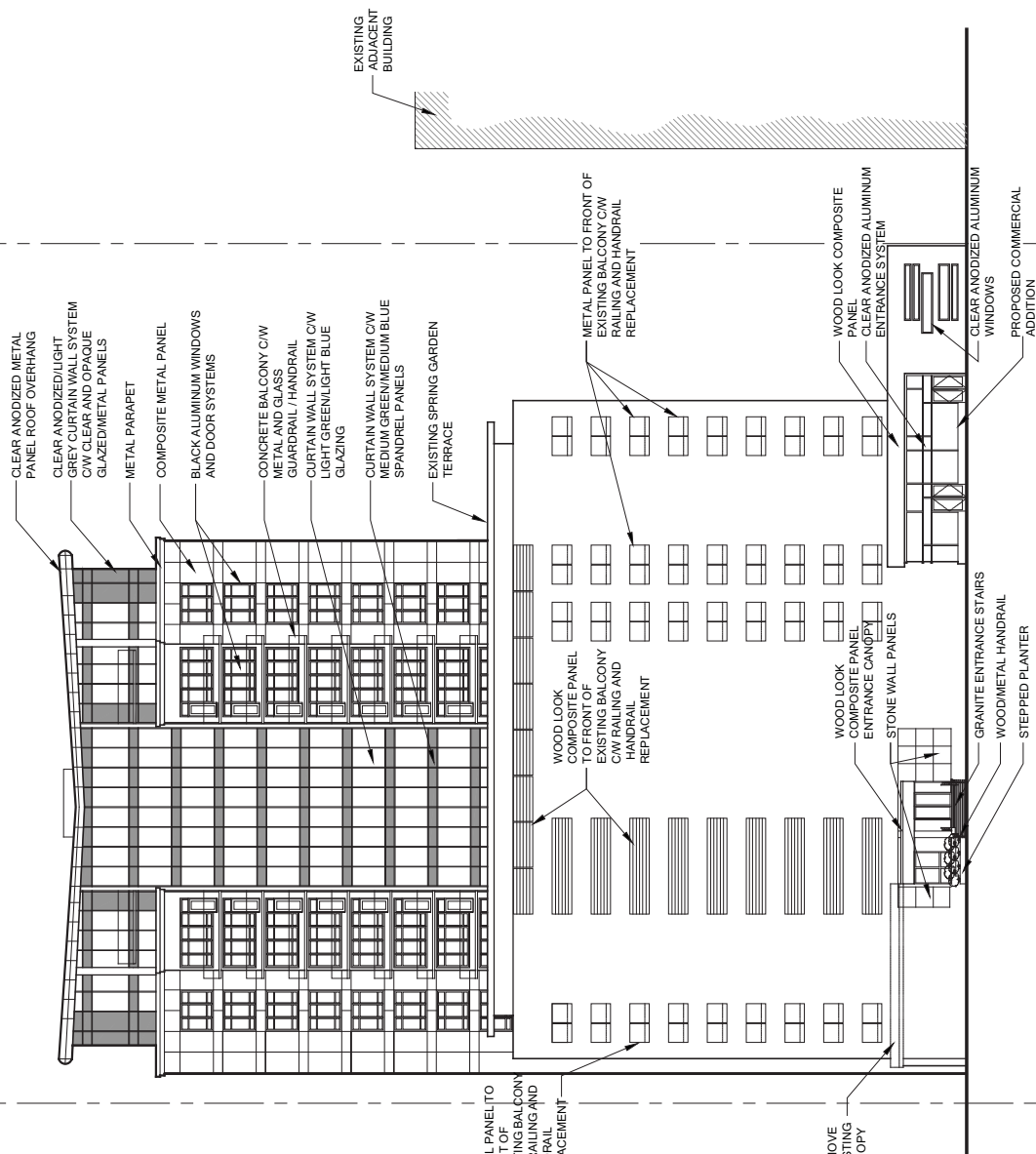
SCALE
AS NOTED

DATE
OCT 19 2012

DRAWING NO.
A-202

PROPERTY LINE

PROPERTY LINE



- 350.03 TOP OF ROOF
- 333.23 LEVEL 20
- 323.31 LEVEL 19
- 313.40 LEVEL 18
- 303.48 LEVEL 17
- 293.56 LEVEL 16
- 283.65 LEVEL 15
- 273.73 LEVEL 14
- 263.81 LEVEL 13
- 253.90 LEVEL 12
- METAL PANEL TO FRONT OF EXISTING BALCONY C/W RAILING AND HANDRAIL REPLACEMENT
- 243.98 LEVEL 11
- 234.06 LEVEL 10
- 224.15 LEVEL 9
- 214.23 LEVEL 8
- 204.31 LEVEL 7
- 194.40 LEVEL 6
- 184.48 LEVEL 5
- 174.56 LEVEL 4
- REMOVE EXISTING CANOPY
- 164.65 LEVEL 3
- 154.73 LEVEL 2
- 146.06 LEVEL MEZZ.
- 139.40 LEVEL 1
- 137.40 LEVEL
- 128.73 LEVEL
- 119.73 LEVEL



CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



KILLAM
 PROPERTIES INC

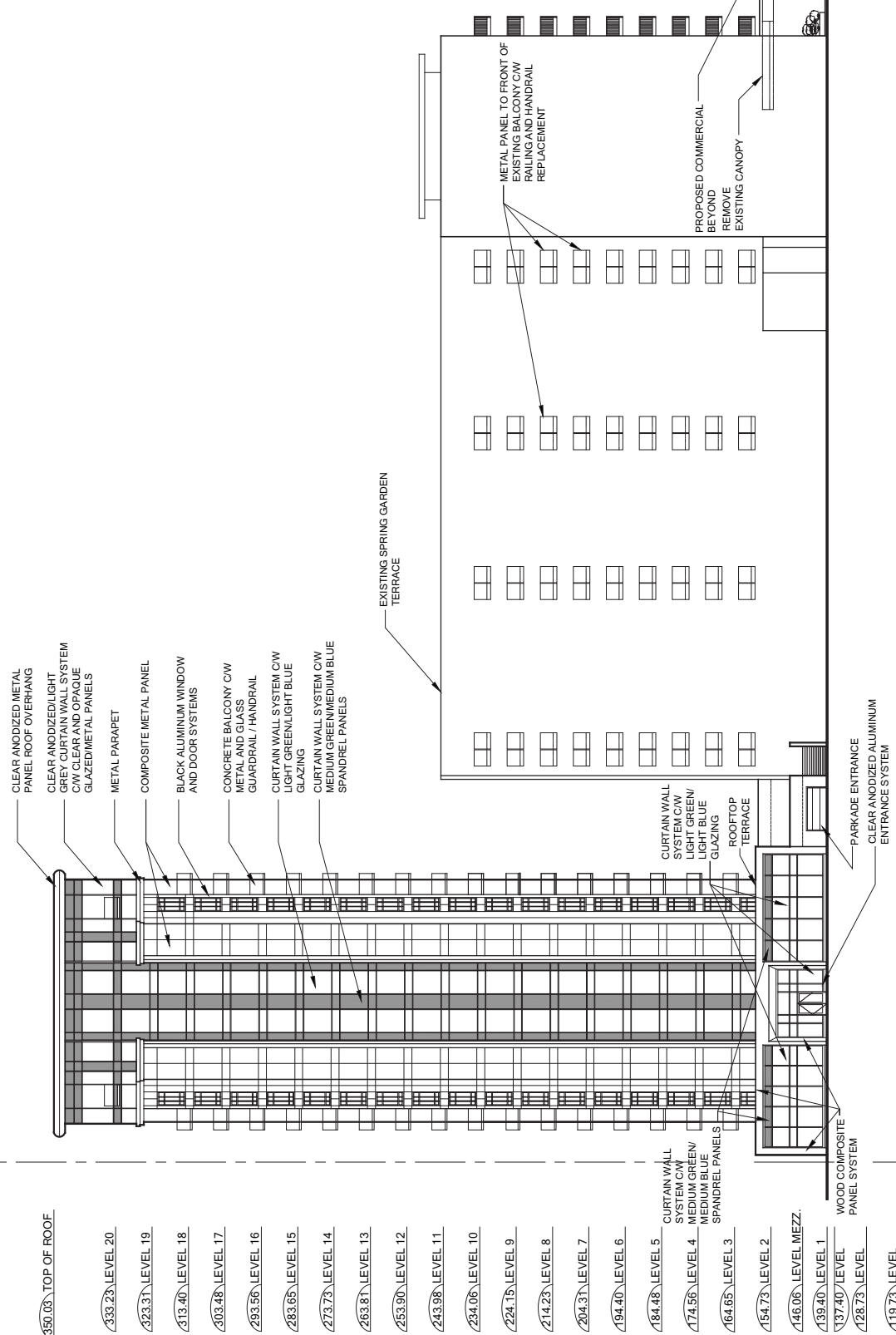
MICHAEL
 APIER
 ARCHITECTURE INC

DRAWING
 SOUTH ELEVATION
 SCALE
 AS NOTED

DATE
 OCT 19 2012
 DRAWING NO.
A-203

PROPERTY LINE

PROPERTY LINE



- CLEAR ANODIZED METAL PANEL ROOF OVERHANG
- CLEAR ANODIZED LIGHT GREY CURTAIN WALL SYSTEM C/W CLEAR ANODIZED GLAZED METAL PANELS
- METAL PARAPET
- COMPOSITE METAL PANEL
- BLACK ALUMINUM WINDOW AND DOOR SYSTEMS
- CONCRETE BALCONY C/W METAL AND GLASS GUARDRAIL / HANDRAIL
- CURTAIN WALL SYSTEM C/W LIGHT GREEN/LIGHT BLUE GLAZING
- CURTAIN WALL SYSTEM C/W MEDIUM GREEN/MEDIUM BLUE SPANDREL PANELS

EXISTING SPRING GARDEN TERRACE

METAL PANEL TO FRONT OF EXISTING BALCONY C/W RAILING AND HANDRAIL REPLACEMENT

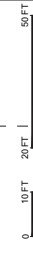
PROPOSED COMMERCIAL BEYOND REMOVE EXISTING CANOPY

CURTAIN WALL SYSTEM C/W LIGHT GREEN/LIGHT BLUE GLAZING

ROOFTOP TERRACE

PARKADE ENTRANCE CLEAR ANODIZED ALUMINUM ENTRANCE SYSTEM

- 350.03 TOP OF ROOF
- 333.23 LEVEL 20
- 323.31 LEVEL 19
- 313.40 LEVEL 18
- 303.48 LEVEL 17
- 293.56 LEVEL 16
- 283.65 LEVEL 15
- 273.73 LEVEL 14
- 263.81 LEVEL 13
- 253.90 LEVEL 12
- 243.98 LEVEL 11
- 234.06 LEVEL 10
- 224.15 LEVEL 9
- 214.23 LEVEL 8
- 204.31 LEVEL 7
- 194.40 LEVEL 6
- 184.48 LEVEL 5
- 174.56 LEVEL 4 MEDIUM GREEN/ MEDIUM BLUE SPANDREL PANELS
- 164.65 LEVEL 3
- 154.73 LEVEL 2
- 146.06 LEVEL MEZZ.
- 139.40 LEVEL 1
- 137.40 LEVEL
- 128.73 LEVEL
- 119.73 LEVEL

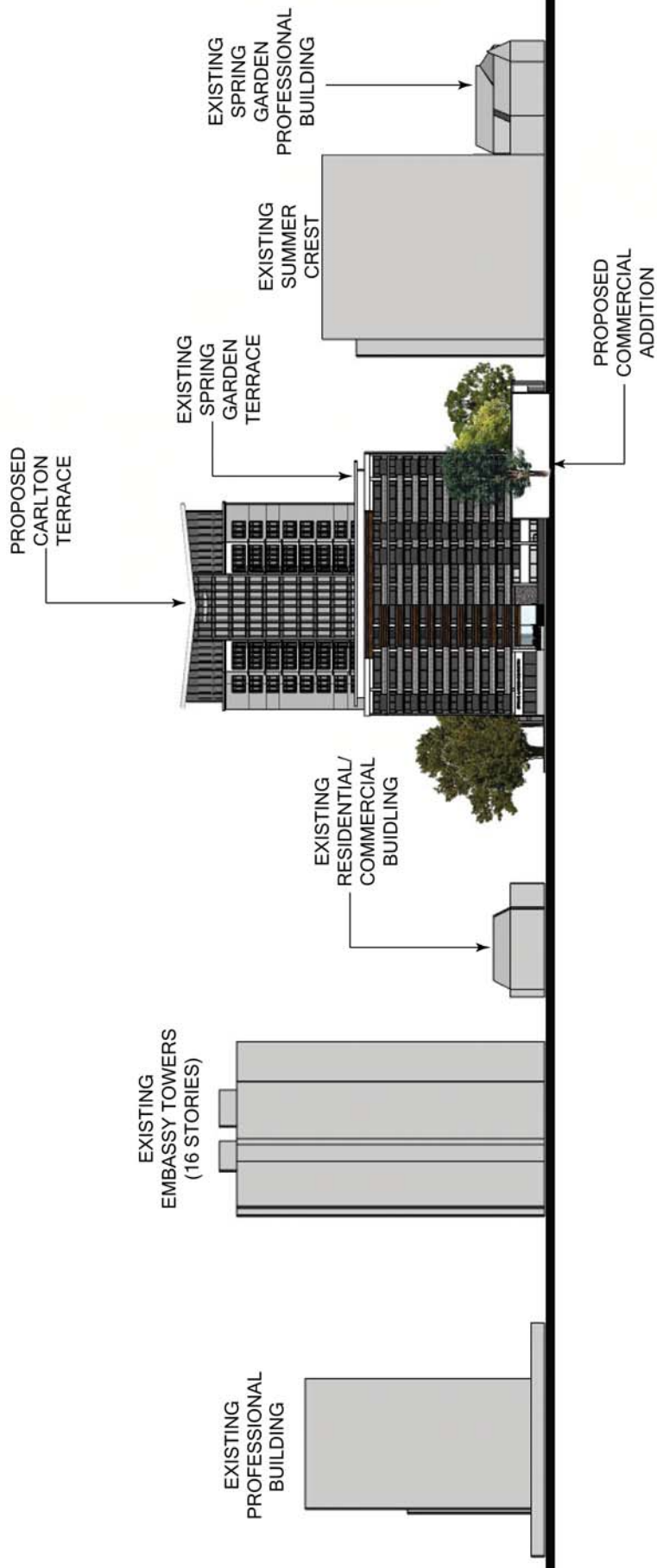


CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



DRAWING WEST ELEVATION
 SCALE AS NOTED

DATE OCT 19 2012
 DRAWING NO. A-204



SECTION VIEW THROUGH SPRING GARDEN ROAD LOOKING NORTH

CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



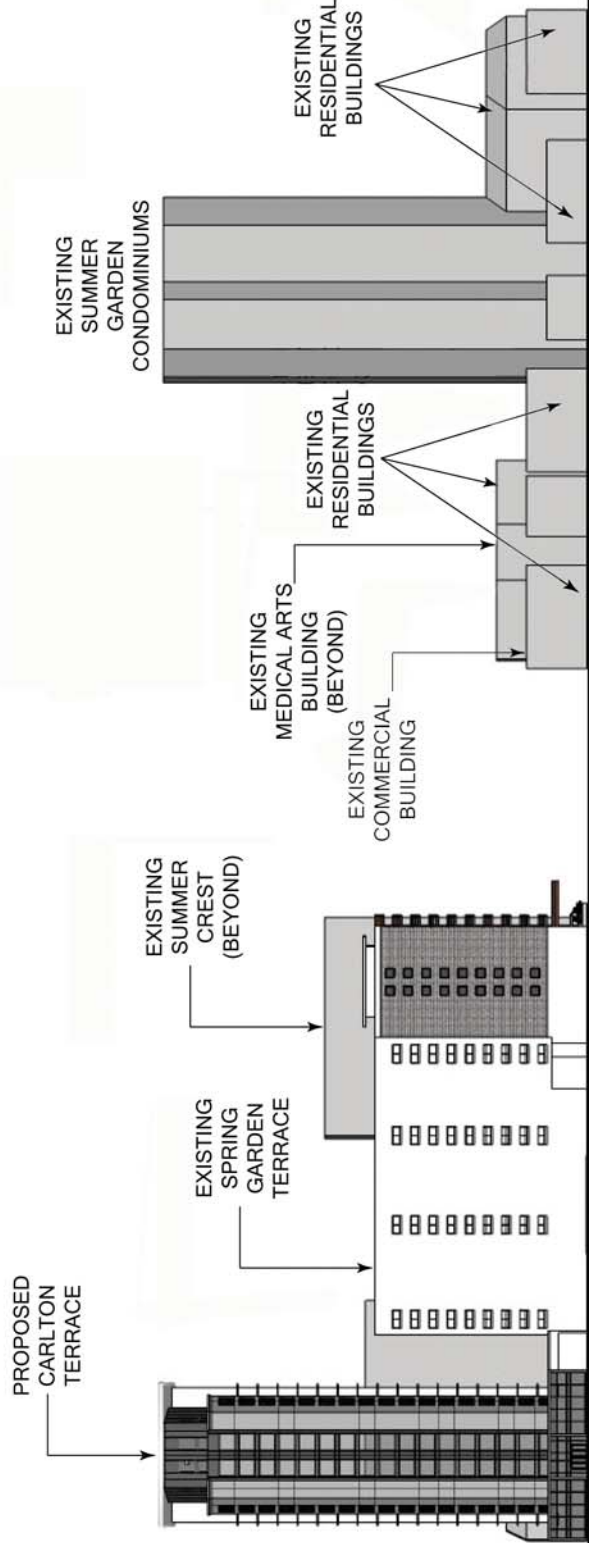
KILLAM
 PROPERTIES INC



MICHAEL
 A PIER
 ARCHITECTURE INC

DRAWING SECTION VIEW
 SCALE AS NOTED

DATE OCT. / 2012
 DRAWING NO A-912



SECTION VIEW THROUGH CARLTON STREET LOOKING EAST

CARLTON TERRACE

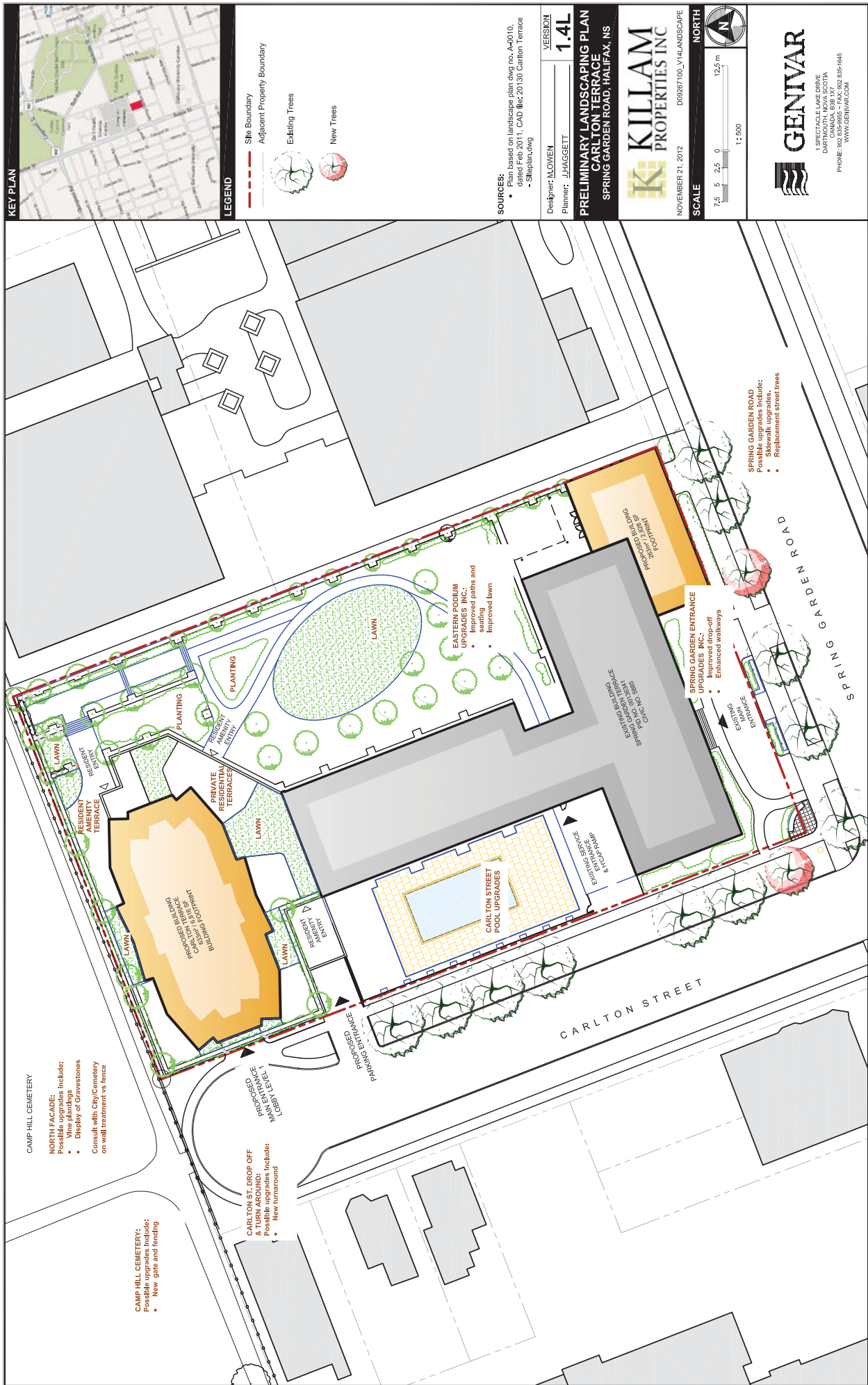
PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



DRAWING
SECTION VIEW
SCALE
AS NOTED

DATE
OCT. / 2012
DRAWING NO.
A-913

Attachment D - Preliminary Landscape Plan



KEY PLAN

LEGEND

- Site Boundary
- Adjacent Property Boundary
- Existing Trees
- New Trees

SOURCES:

- Plan based on landscape plan dwg no. A-0010, dated Feb 2011, CAD file: 20130 Carlton Terrace - Shepleaf.dwg

Designer: M.OWEN
Planner: J.HAGGETT

VERSION
1.4L

PRELIMINARY LANDSCAPING PLAN
CARLTON TERRACE
SPRING GARDEN ROAD, HALIFAX, NS

KILLAM PROPERTIES INC.
NOVEMBER 21, 2012
D06267100_Y14LANDSCAPE

SCALE
7.5 5 2.5 0 12.5 m
1:500

NORTH

GENIVAR
1 SPECTACLE LAKE DRIVE
DARTMOUTH, NS B3A 2T4
CANADA, B3B 1X7
PHONE: 902-855-9555 • FAX: 902-852-1845
WWW.GENIVAR.COM

CAMP HILL CEMETERY
NORTH FACADE:
Possible upgrades include:
• Display of Gravestones
Consult with City/Cemetery on wall treatment vs fence

CAMP HILL CEMETERY:
Possible upgrades include:
• New gate and fencing

CARLTON ST. DROP OFF
PARKING ENTRANCE
PROPOSED 1 LEVEL PARKING LOT
Possible upgrades include:
• New turnaround

EASTERN PODIUM
UPGRADES INC.
Improved paths and
landscaping
• Improved lawn

CARLTON STREET
POOL UPGRADES
• Improved drop-off
• Enhanced walkways

SPRING GARDEN ENTRANCE
UPGRADES INC.
Improved drop-off
• Enhanced walkways

SPRING GARDEN ROAD
Possible upgrades include:
• Sidewalk upgrades
• Replacement street trees

Attachment E - Shadow Study



5540 Kaye Street
Halifax, Nova Scotia
B3K 1Y5

Tel 902 455 5522
Fax 902 455 5523

October 19, 2012

Planning Applications
Planning & Development Services
PO Box 1749
Halifax, NS
B3J 3A5

Attention: Kurt Pyle

**Re: Plan Amendment / Development Agreement Application – Proposed Residential Tower,
Carlton Street, Halifax
Shadow Impact Statement**

The proposed project is situated at the end of Carlton Street, bound on the North side by Camp Hill Cemetery. It is a proposed extension to 'Spring Garden Terrace', which is located on the corner of Spring Garden Road and Carlton Street.

Neighboring properties include the Camp Hill Cemetery to the North, the 11 storey 'Spring Garden Terrace' building to the South, the 16 storey 'Embassy Towers' to the West, and the 12 storey 'Summer Crest' complex to the East. Further East beyond 'Summer Crest' is the Public Gardens.

On its North-South axis, the base of the proposed building extends from the Camp Hill Cemetery to the existing 'Spring Garden Terrace'. On its East-west axis, the building's base extends to the eastern property line and is set back 20'-0" from Carlton Street to the West. The height of the base is 19'-6" before it steps back to the tower above.

The tower of the proposed building rises 20 stories to a height approximately 210'-0" above grade. The tower is setback approximately 6'-6" from Carlton Street property line to the East, 8'-0" above from Camp Hill Cemetery to the North, 72'-0" above from the eastern property line, and 28'-0" above from the property line with 'Spring Garden Terrace' to the South.

The effects of the buildings' shadows on the adjacent neighborhood were analyzed through computer modeling. Four observation periods were recorded through time animation. Three times were analyzed at the Winter Solstice (Dec 21), and four times were analyzed at the Summer Solstice (Jun 21) - the least and most intrusive time periods respectively. Four times were analyzed at the Spring and Fall Equinox (Mar 21 and Sept 22) - the mid-points in between. The individual observation periods noted below started at sunrise and were recorded at fifteen minute intervals until sunset.

The effects of this building on the shading of Carlton Street and the opposite property are minimal. On March 21 and September 22 there is virtually no shadow from the proposed building on either Carlton Street or 'Embassy Towers'. On December 21 at sunrise, one-half the length of Carlton Street between Spring Garden Road and Camp Hill Cemetery is in shadow, generated almost entirely from the existing 'Spring Garden Terrace' building with minimal additional impact caused by the proposed new residential project. The shadow coverage diminishes in size until it leaves the street

completely at 10:00am. On June 21 the shadow occupies the very end of Carlton Street beginning at sunrise and leaves the street completely by 9:30 am.

Portions of Camp Hill Cemetery – predominantly the southern half - are periodically cast in the shadow of the proposed building. On December 21, the shadow sweeps across the southern portion of the cemetery where it ultimately comes to rest, from 2:00 pm until sunset, in a small portion of its South-east corner. On June 22, a minimal shadow enters the cemetery in its South-west corner, remains in the southern 1/3 of the cemetery, travelling to the South-east corner where it leaves the cemetery at 2:30 pm. March 21 and September 22 have similar shadow patterns to June, the main difference seeing the shadow leave the cemetery at 4:15 pm and 4:00 pm respectively.

The 'Summer Crest Building' building has shadow cast upon it starting at 1:30 pm on June 22, 2:45 pm on March 21, and 2:30 pm on September 22. 'Summer Crest' experiences shading between 2:15 pm and 4:00 pm during all observation periods. After 4:00 pm this building is in shadow until sunset with exception on June 22, where it leaves the shadow at 6:45 pm.

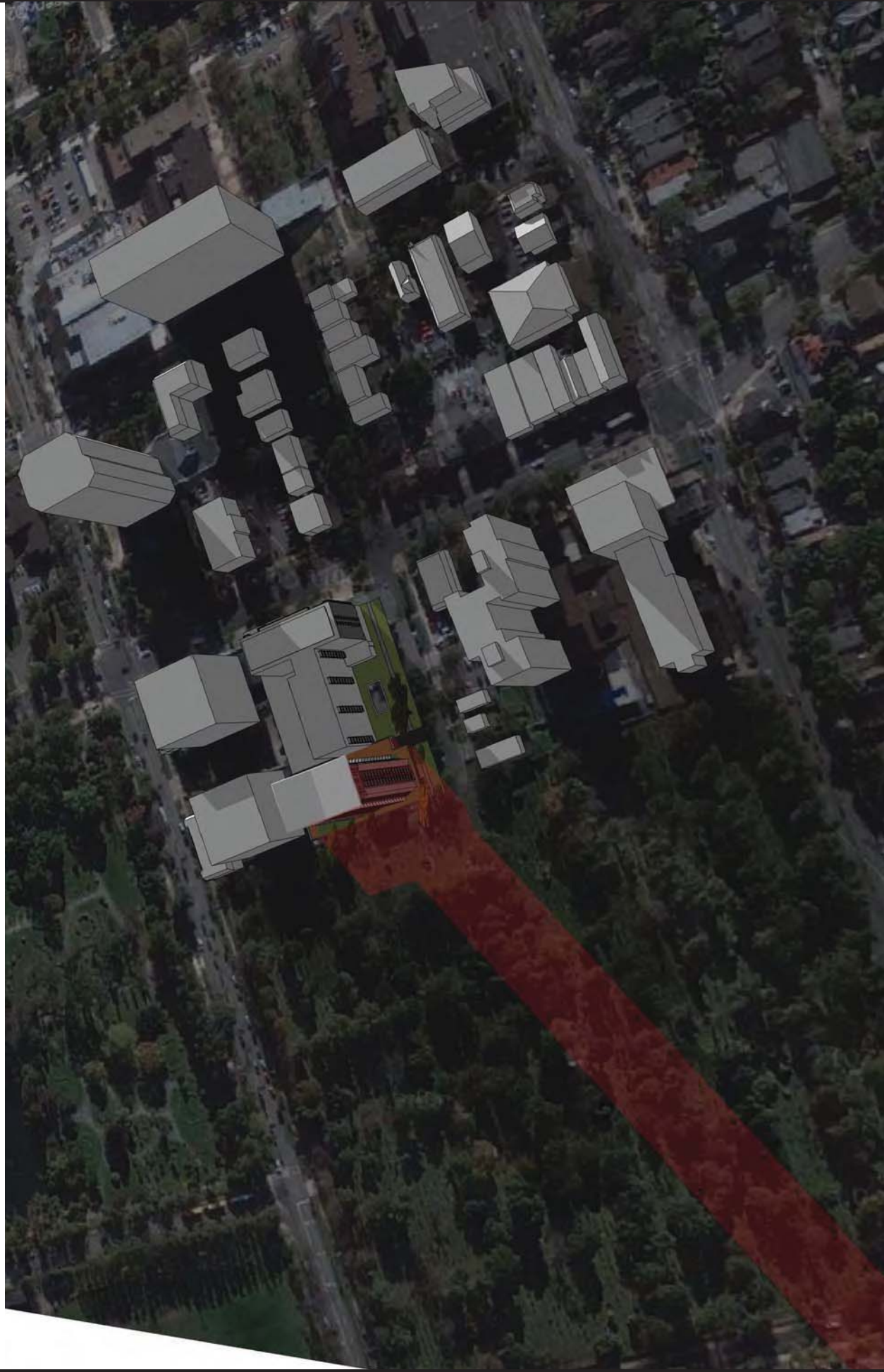
The proposed building casts an afternoon shadow on limited portions of The Public Gardens during all four observations periods. On December 21, the shadow touches the North-west corner of the Gardens at 2:00 pm and extends across the North side of the gardens to reach South Park Street as the afternoon progresses. On June 22 at 6:00 pm the building's shadow touches a small area in the South-west corner of the Gardens and remains here until sunset. On March 22 and September 22 a very narrow shadow sweeps from the center of the Gardens to the South-east corner beginning at 3:15 pm and 3:00 pm respectively.

Through careful and studied design of the massing and placement on its site the designers have been able to achieve a building which will contribute little to the diminishment of solar penetration in this neighbourhood.

Regards,
A

Original signed

Michael Napier NSAA AANB MRAIC



DECEMBER 21 - 9am

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

DATE

OCT. 19, 2012

DRAWING NO.

A-901

SCALE

AS NOTED



DECEMBER 21 - 12pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

DATE

OCT. 19, 2012

SCALE

AS NOTED

DRAWING NO.

A-902



DECEMBER 21 - 3pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC

MZA
MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

DATE

OCT. 19, 2012

SCALE

AS NOTED

DRAWING NO.

A-903



JUNE 20 - 9am

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC

MZA
MICHAEL
APIER
ARCHITECTURE INC

DRAWING
SHADOW STUDY

SCALE
AS NOTED

DATE
OCT. 19, 2012

DRAWING NO.
A-904



JUNE 20 - 12pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

DATE

OCT. 19, 2012

SCALE
AS NOTED

DRAWING NO.

A-905



JUNE 20 - 3pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

SCALE
AS NOTED

DATE

OCT. 19, 2012

DRAWING NO.

A-906



JUNE 20 - 6pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

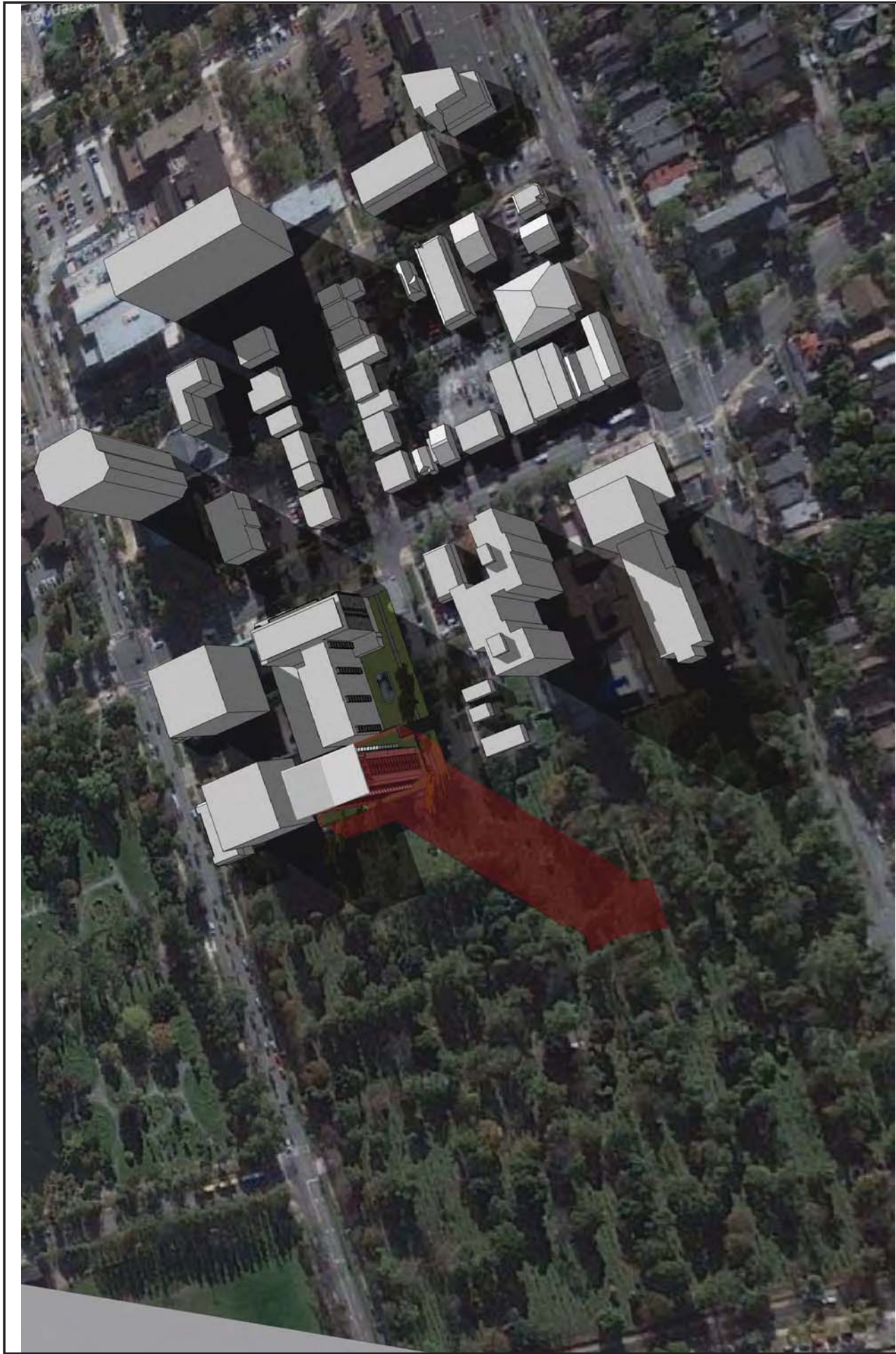
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OCT. 19, 2012

DRAWING NO.

A-907



MARCH 20 - 9am

CARLTON TERRACE
PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



DRAWING
SHADOW STUDY
SCALE
AS NOTED

DATE
OCT. 19, 2012
DRAWING NO.
A-908



MARCH 20 - 12pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

DATE

OCT. 19, 2012

SCALE

AS NOTED

DRAWING NO.

A-909



MARCH 20 - 3pm

CARLTON TERRACE

PROPOSED DEVELOPMENT
HALIFAX, NOVA SCOTIA



KILLAM
PROPERTIES INC



MICHAEL
APIER
ARCHITECTURE INC

DRAWING

SHADOW STUDY

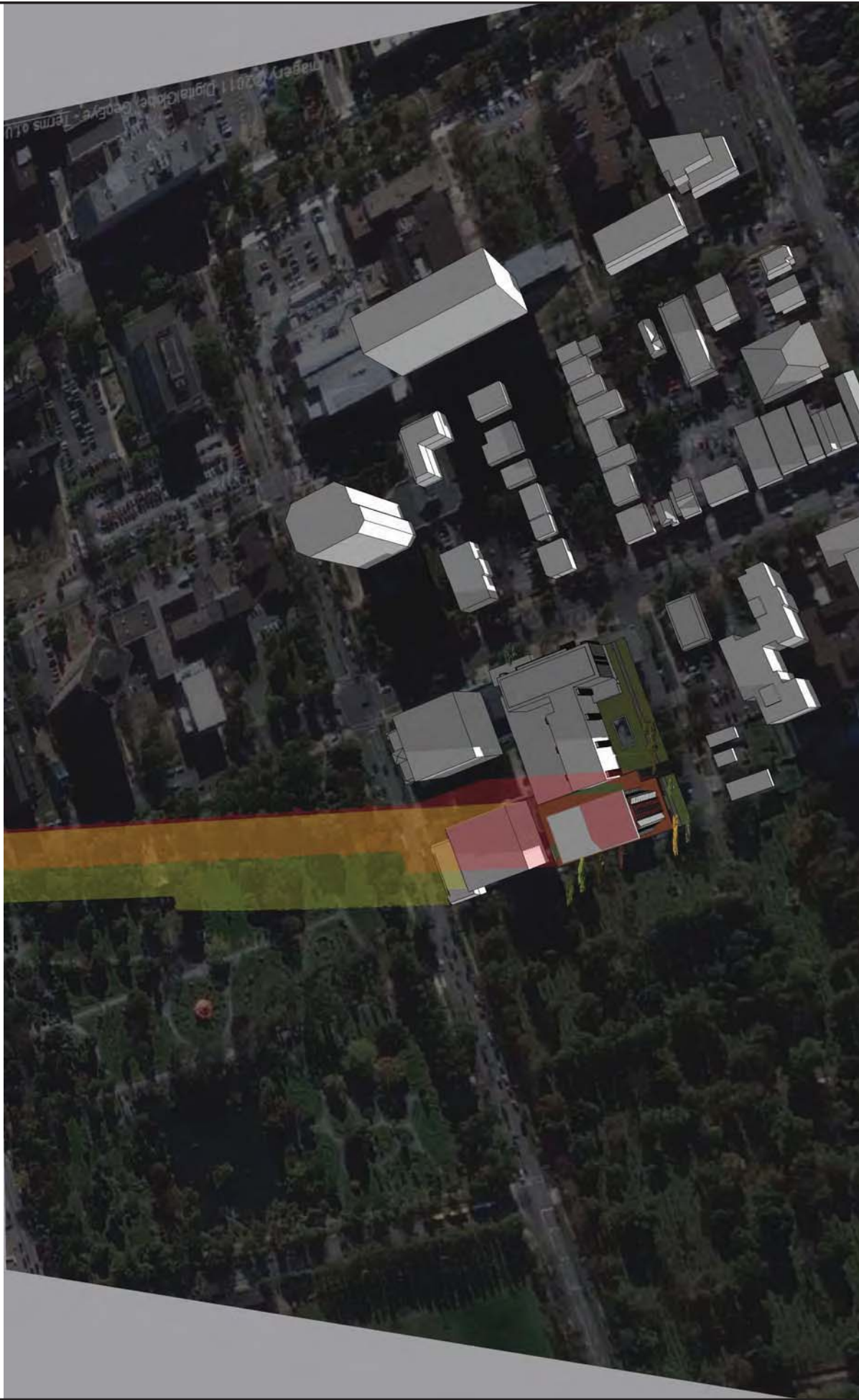
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OCT. 19, 2012

DRAWING NO.

A-910



MARCH 20 - 6pm

CARLTON TERRACE
 PROPOSED DEVELOPMENT
 HALIFAX, NOVA SCOTIA



KILLAM
 PROPERTIES INC



MICHAEL
 APIER
 ARCHITECTURE INC

DRAWING
 SHADOW STUDY
 SCALE
 AS NOTED

DATE
 OCT. 19, 2012
 DRAWING NO.
A-911

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Carlton Terrace
Halifax, Nova Scotia

Pedestrian Wind Assessment

RWDI # 1400500
November 8, 2013

SUBMITTED TO

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1. INTRODUCTION

Rowan Williams Davies & Irwin Inc. (RWDI) was retained by Michael Napier Architecture to assess the wind comfort conditions for the proposed Carlton Terrace development located at the north end of Carlton Street in Halifax, Nova Scotia (see Image 1).

The objective of this assessment is to provide a qualitative evaluation of pedestrian wind conditions around the proposed development. Conceptual mitigation measures to improve wind comfort are provided, where necessary. This qualitative assessment is based on the following:

- a review of regional long-term meteorological data;
- previous wind studies, undertaken by RWDI, in the Halifax area;
- design drawings and renderings received by RWDI on October 15 and 21, 2013;
- our engineering judgment and knowledge of wind flows around buildings;
- our experience of wind tunnel studies of various building projects¹⁻³, and,
- use of software developed by RWDI (Windestimator²) for estimating the potential wind conditions around generalized building forms.

This qualitative approach provides a screening-level estimation of potential wind conditions. To quantify these conditions or refine any conceptual mitigation measures, physical scale model tests would typically be required.

Note that other wind issues, such as those relating to door pressures, cladding/structure loads, stack effect, exhaust re-entrainment, snow drifting, etc. are not considered in the scope of the assessment.

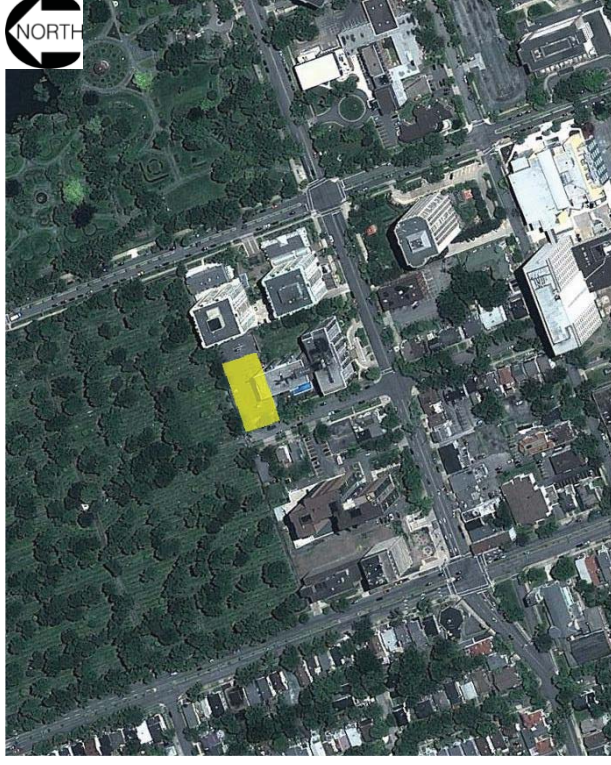


Image 1 – Aerial Photo of Site and Surroundings
(courtesy of Google Earth™)

1. C.J. Williams, H. Wu, W.F. Waechter and H.A. Baker (1999), "Experience with Remedial Solutions to Control Pedestrian Wind Problems", *10th International Conference on Wind Engineering*, Copenhagen, Denmark.
2. H. Wu, C.J. Williams, H.A. Baker and W.F. Waechter (2004), "Knowledge-based Desk-Top Analysis of Pedestrian Wind Conditions", *ASCE Structure Congress 2004*, Nashville, Tennessee.
3. H. Wu and F. Kriksic (2012). "Designing for Pedestrian Comfort in Response to Local Climate", *Journal of Wind Engineering and Industrial Aerodynamics*, vol.104-106, pp.397-407.

2. SITE INFORMATION

The proposed development site is located at the north end of Carlton Street in Halifax. It would be a 20-storey residential building with a total height of 64m (Image 2). It would have a rectangular plan with the long axis in an east-west direction, as shown in Image 3. The proposed development would also consist of a one-storey addition at the southeast corner of the existing Spring Garden Terrace building (Image 3).

The key pedestrian areas would include the main entrances to the proposed and existing buildings, public sidewalks and amenity spaces around the proposed Carlton Terrace and the existing Spring Garden Terrace (see Image 3).

Adjacent to the site there are several large residential and commercial buildings in the range of 10 to 20 storeys in height (Images 2 and 4). There is a cemetery to the north and a public park to the northeast and east (Image 1). To the further northeast and east are the Citadel, Downtown Halifax and the Harbour.

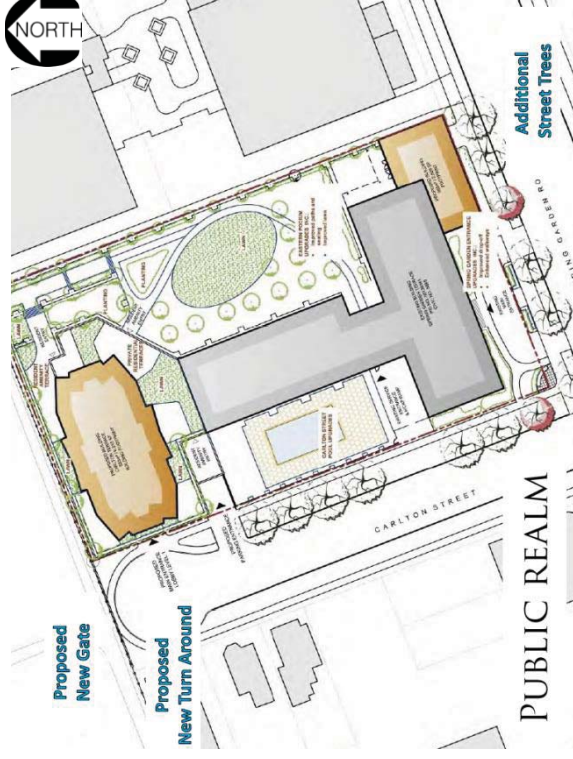


Image 3 – Project Site Plan

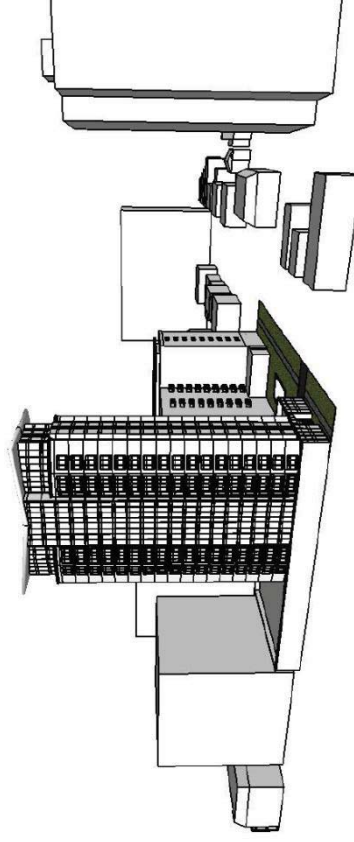


Image 2 – Perspective View from Northwest

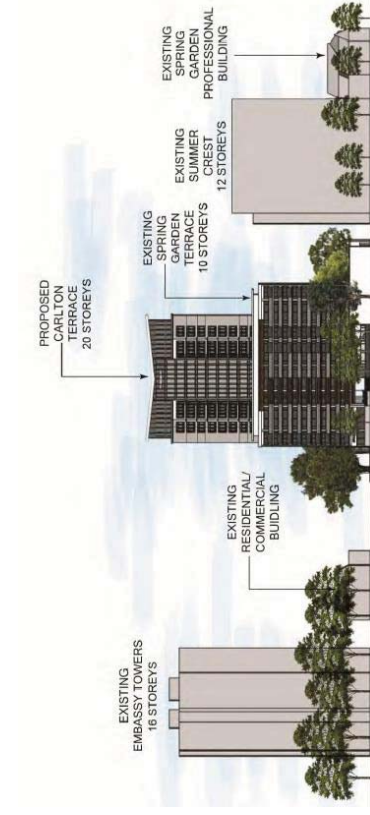


Image 4 – View from Spring Garden Road

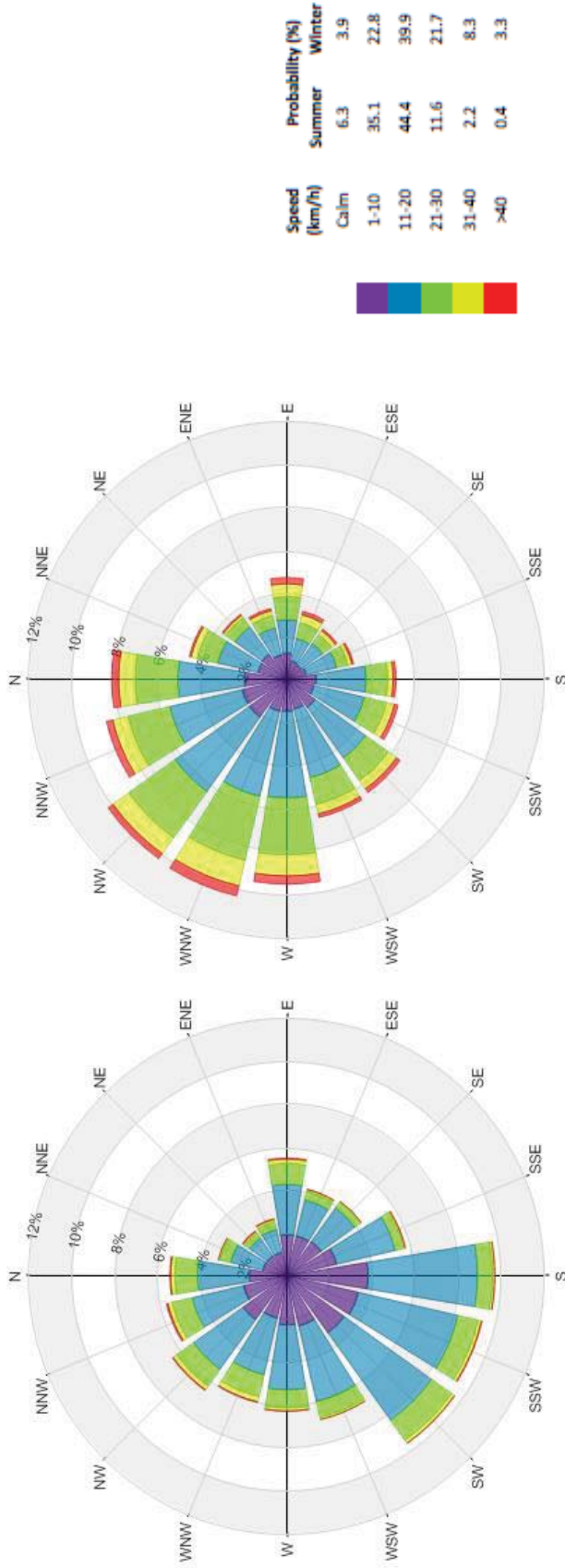
3. METEOROLOGICAL DATA

Meteorological data from Shearwater Airport in Dartmouth, NS for the period of 1980 to 2009 were used as reference for wind conditions in the region.

The distributions of wind frequency and directionality for summer (May through October) and winter (November through April) seasons are shown in the wind roses below (Image 5). Winds from the southwest quadrant are predominant in the summer, as indicated by the left hand wind rose; secondary winds are from the northwest quadrant. The right hand wind rose shows the winter data, indicating the predominance of winds from the northwest quadrant.

Strong winds of a mean speed greater than 30 km/h measured at the airport occur for 2.6% and 11.6% of the time during the summer and winter seasons, respectively. The northwest quadrant winds are prevalent throughout the year. Winds from these directions potentially could be the source of uncomfortable or even severe wind conditions, depending upon the site exposure or development design.

Winds approaching from the northwest, west, southwest, and south were considered most important for this assessment; however, winds from all directions were considered in the numerical assessment of pedestrian wind conditions at the site.



Summer Winds (May to October)

Winter Winds (November to April)

Image 5 - Directional Distribution (%) of Winds (Blowing From) Shearwater Airport (1980 to 2009)

4. EXPLANATION OF CRITERIA

The RWDI pedestrian wind comfort criteria deal with both pedestrian safety and comfort, as they relate to the force of the wind. Thermal effects (e.g., temperature, humidity, sun/shade, etc.) are not considered in these comfort criteria. These criteria have been developed by RWDI through research and consulting practice since 1974. They have also been widely accepted by municipal authorities as well as by the building design and city planning community. The pedestrian wind comfort criteria are categorized as follows:

- **Sitting:** Low wind speeds during which one can read a newspaper without having it blown away. These wind speeds are appropriate for outdoor cafes and other amenity spaces that promote long term sitting.
- **Standing:** Slightly higher wind speeds that are strong enough to rustle leaves. These wind speeds are appropriate at major building entrances, bus stops or other areas where people may linger but not necessarily sit for extended periods of time.
- **Strolling:** Moderate winds that would be appropriate for window shopping and strolling along a downtown street, plaza or park.
- **Walking:** Relatively high speeds that can be tolerated if one's objective is to walk, run or cycle without lingering.

Wind conditions are considered suitable for sitting, standing, strolling or walking if the wind speeds are expected for at least four out of five days (80% of the time). An **uncomfortable** designation means that the criterion for walking is not satisfied.

Safety is also considered by the criteria and is associated with excessive gust wind speeds that can adversely affect a pedestrian's balance and footing. If winds sufficient to affect a person's balance occur more than 0.1% of the time or 9 hours per year, the wind conditions are considered severe. Wind control measures are typically required at locations where winds are rated as uncomfortable or they exceed the wind safety criterion.

These criteria for wind forces represent average wind tolerance. They are sometimes subjective and regional differences in wind climate and thermal conditions as well as variations in age, health, clothing, etc. can also affect people's perception of the wind climate.

5. PEDESTRIAN WIND CONDITIONS

5.1 Background

Predicting wind speeds and occurrence frequencies is complicated. It involves building geometry, orientation, position and height of surrounding buildings, upstream terrain and the local wind climate. Over the years, RWDI has conducted more than 2,000 wind-tunnel model studies on pedestrian wind conditions around buildings, yielding a broad knowledge base. This knowledge has been incorporated into RWDI's proprietary software that allows, in many situations, for a qualitative, screening-level numerical estimation of pedestrian wind conditions without wind tunnel testing.

Generally, wind conditions suitable for walking or strolling are appropriate for sidewalks and parking lots; lower wind speeds comfortable for standing are preferred at major building entrances. It is desirable for wind conditions on outdoor terraces to be comfortable for sitting more than 80% of the time in the summer, when these areas are in use.

Tall buildings tend to intercept the stronger winds at higher elevations and redirect these flows down to grade. Such a *Downwashing Flow* (Image 6a) is often the main cause for wind flow accelerations around tall buildings at the pedestrian level. An effective measure to reduce the direct impact of the downwashing flow is to include a large podium around the tower (Image 6b). Also, when two buildings are situated side by side, wind flows tend to accelerate through the space between the buildings due to the *Channelling Effect* (Image 6c). If these building/wind combinations occur for prevailing winds, there is a greater potential for increased wind activity.

The proposed podium around the tower (Images 2 and 3) is a positive design feature for reducing the impact of the downwashing flows on the ground. The chamfered tower corners tend to promote horizontal wind flows and reduce the downwashing (Image 3). Additional tower setback from the Carlton Street would further improve the wind conditions at the main entrance and along the adjacent sidewalks.

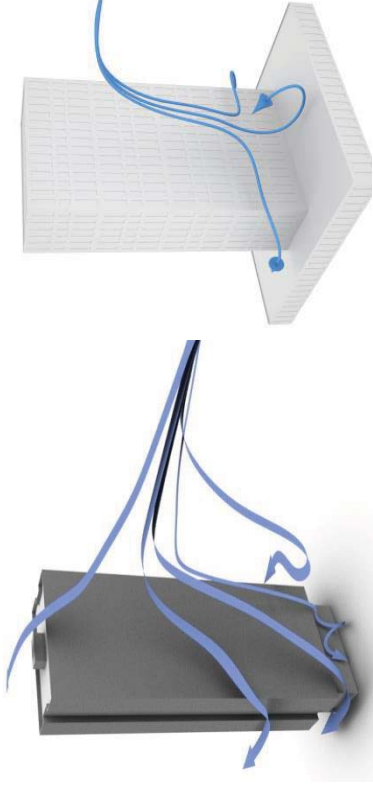


Image 6a – Downwashing Flow

Image 6b – Large podium for Wind Control

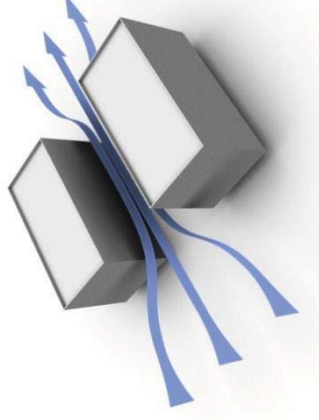


Image 6c – Channelling Effect

The flow patterns for the prevailing northwest and southwest winds are illustrated below (Image 7). Detailed discussions of potential wind conditions are provided in the following sections for key pedestrian areas on and around the development. Where necessary, mitigation concepts are provided to improve the wind conditions.

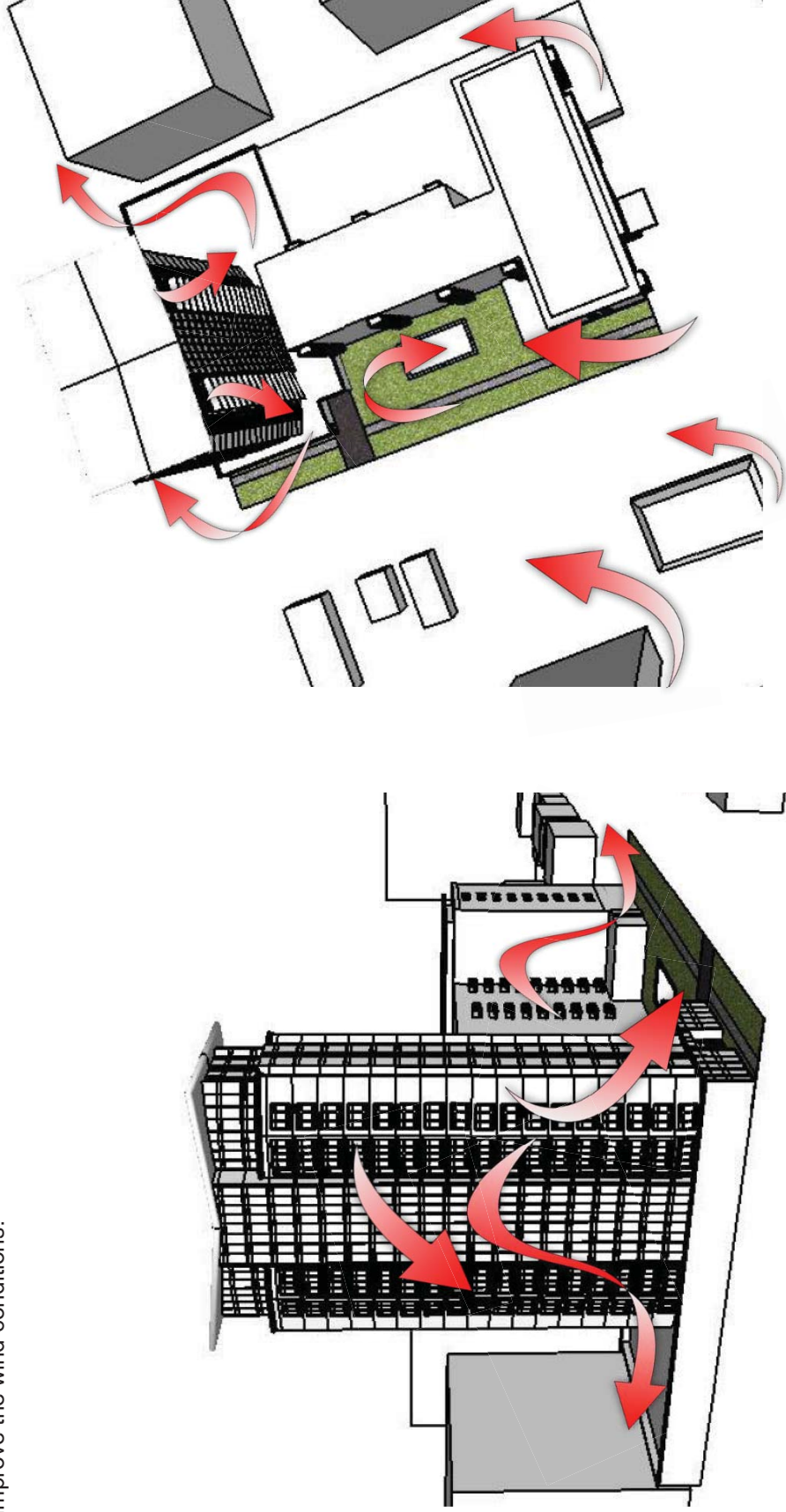


Image 7 – Flow Patterns for Winds from Northwest (left) and Southwest (right)

5.2 Building Entrances

The main entrance to the proposed building would be recessed from the west façade (Location A1 in Image 8), which is a positive design feature for wind control. The tower setback is also a positive feature to reduce the downwashing. However, due to its exposure to the prevailing northwest and southwest winds, the area in front of the entrance would be windy from time to time. A large canopy above the entrance would further reduce the wind downwashing, and landscaping on both sides of the entranceway and along the sidewalk would also be beneficial for wind reduction.

Wind conditions at the main entrance to the existing Spring Garden Terrace (Location A2 in Image 9) would not be negatively affected by the proposed tower or the one storey addition to the east. The entrance is sheltered by the existing building from the prevailing northwesterly winds. The addition of a large canopy would also shelter the entrance area from the south and southwest winds that may downwash off of the south façade. Dense landscaping around the southwest corner of the building (Image 9) would further improve the wind conditions in the entrance area.

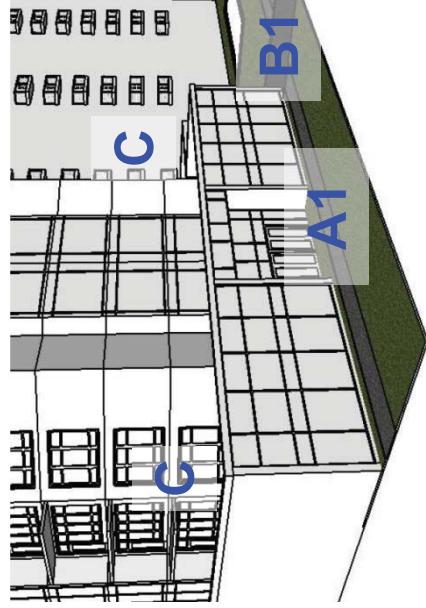


Image 8 – Recessed Main Entrance

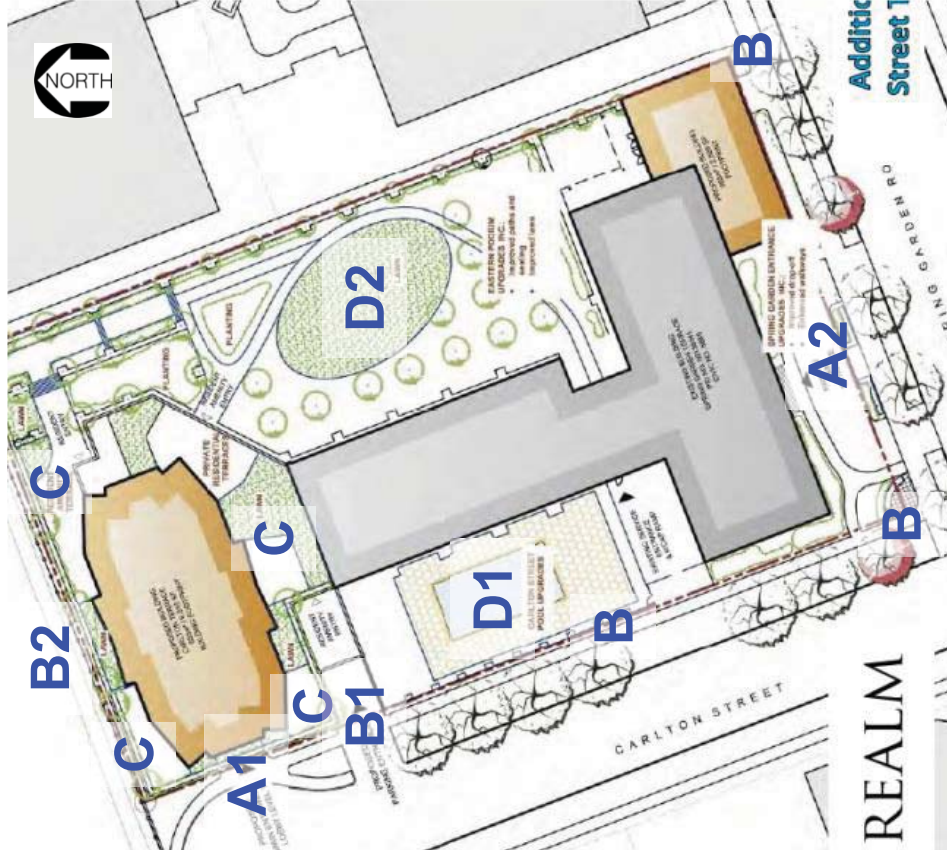


Image 9 – Reference Plan

5.3 Sidewalks

Wind speeds along the sidewalks on Carlton Street and Spring Garden Road would not be affected by the proposed development in general, and they are expected to be comfortable for walking or better throughout the year.

One exception is the sidewalk west of the new tower, especially around the southwest corner of the development (Location B1 in Image 9), where increased wind speeds are expected, due to the winds being deflected down by the proposed 20-storey building. With the addition of the proposed building, wind speeds at B1 could become uncomfortable from time to time in the winter. If further recessing the tower from the west property line is not feasible, coniferous or marcescent landscaping should be considered along the sidewalk, especially to the north of Location B1.

Increased wind speeds are also expected on the north side of the tower (Location B2 in Image 9). The resultant wind conditions would be uncomfortable in the winter, but this is not a concern due to limited pedestrian use of the area.

5.4 Podium around the New Tower

The podium area around the proposed tower (Locations C) would be affected by the winds downwashing off of the tower façade and channelling between the proposed and existing buildings. The resulted wind speeds are expected to be higher than desired for passive pedestrian activities in the summer. In the winter, the wind conditions would be uncomfortable and exceed the safety criterion at the exposed building corners.

Wind control measures, such as trellises, landscaping and tall parapets, are recommended for the podium area to create a comfortable wind environment. Photo examples are shown in Image 10. In the winter, access to the podium area around the tower should be restricted.

If desired, wind tunnel tests can be conducted to quantify the wind conditions and to evaluate wind control solutions.

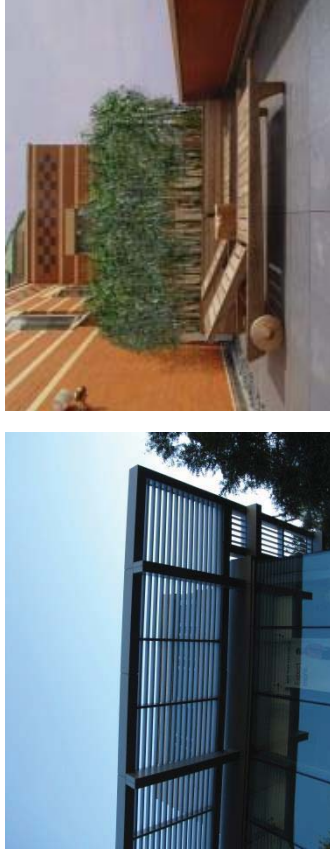


Image 10 – Examples of Wind Control Measures

5.5 Amenity around Spring Garden Terrace

There is a swimming pool on the west side of the existing 10-storey Spring Garden Terrace (Location D1 in Image 9) and a large amenity area on the east side (Location D2).

The swimming pool is currently exposed to the prevailing southwest and northwest winds. The proposed tower would deflect more northwest winds to the pool area, but shelter the pool from the northerly winds. The creation of a large massing to the north of the pool would induce a back pressure when winds are from the southerly and southwesterly directions, which slows down the wind speed over the pool area.

Overall, the existing wind activity in the pool area would remain similar after the construction. Additional landscaping and taller fences may be considered around the pool area to improve the wind conditions in the summer.

The amenity area on the east side of the building (Location D2) is sheltered by the existing Spring Garden Terrace from the southwesterly winds. The proposed tower would provide additional sheltering from the northwesterly winds. The wind conditions in this amenity area are expected to be improved from those that currently exist.

6. SUMMARY

The proposed development includes several positive design features for wind control, including tower setback (podium), chamfered tower corners, recessed main entrances and landscaping. Due to the surrounding topography and location of pedestrian areas, we predict suitable wind conditions at building entrances, sidewalks and existing amenity spaces in general.

Higher wind speeds are predicted along sidewalks on the west side of the proposed development, in the area north of the tower and on the podium areas immediately around the tower. Wind control measures are discussed and photo examples are provided for reference.

If desired, wind tunnel tests can be conducted to quantify these wind conditions and to develop wind control solutions.

7. APPLICABILITY OF RESULTS

In the event of any significant changes to the design, construction or operation of the building or addition of surroundings in the future, RWDI could provide an assessment of their impact on the design considered in this report. It is the responsibility of others to contact RWDI to initiate this process.



Ref. No. DA09267

November 21, 2012

Michael McLean
Vice President Developments
Killam Properties
3700 Kempt Road, Suite 100
HALIFAX NS B3K 4X8

**RE: Traffic Impact Statement - Proposed Carlton Terrace Apartment Building
Carlton Street, Halifax, NS**

Dear Mr. McLean:

Plans are being prepared for construction of a 110 unit apartment building at the north end of Carlton Street, Halifax (Figure 1). The proposed development also includes an approximately 2,800 square foot building to be used as a restaurant abutting Spring Garden Road at the east site boundary. Construction is expected to start during 2014 with the building being completed in 2017. This is the Traffic Impact Statement required for making development applications to Halifax Regional Municipality (HRM).

Site Description - The site, which is at the north end of Carlton Street is currently used as a ground level parking lot at the north end of Spring Garden Road Civic No. 5885. The site is bounded by the existing Civic No. 5885 building to the south and Camp Hill Cemetery to the north (Figure 1). Site access will be from driveways to Carlton Street.

Street Descriptions - Carlton Street adjacent to the site is a short dead end street with Camp Hill Cemetery to the north and a STOP sign controlled four-way intersection with Spring Garden Road to the south (Photo 1). Carlton Street, which intersects with Spring Garden Road one block east of Robie Street, is two lanes wide with sidewalks and metered parking spaces on both sides. All site generated trips will access Spring Garden Road at the Carlton Street intersection.

Spring Garden Road is a collector street with two through lanes in each direction, and bus stops and parking in the curb lane. There is a pedestrian crosswalk with flashing amber beacons across Spring Garden Road immediately east of the intersection.



Photo 1 - Looking north on Carlton Street from the south side of Spring Garden Road. The development site is immediately behind the brick building at the right of the photo.

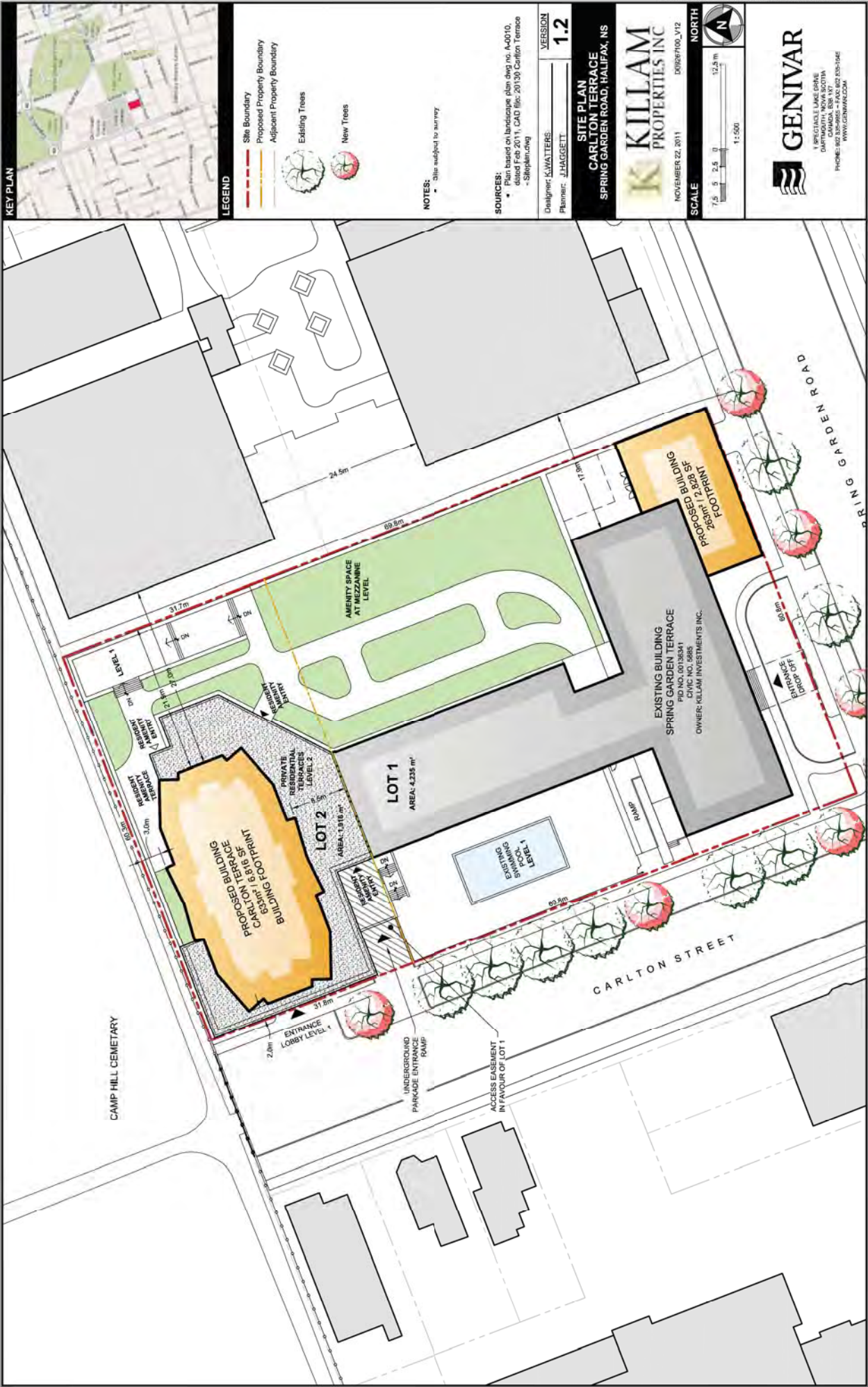


Figure 1

Development Description - The existing development includes a 201 unit apartment building with 157 parking spaces (60 surface and 97 underground spaces). The existing building has about 3600 square feet of ground floor commercial space which is occupied by a dental practice. The proposed development (Figure 1) will include construction of a 110 unit apartment building on the surface parking lot at the north end of the site and a 2,828 square foot commercial building to used as a sit-down restaurant abutting Spring Garden Road at the south east corner of the site. The proposed development will include an additional 121 underground parking spaces to replace the 60 surface spaces now on the site, for a total of 218 underground parking spaces to serve the existing and proposed development. The number of metered parking spaces on Carlton Street is not expected to be affected by the development. Site access is proposed at driveway entrances on Carlton Street.

Traffic Volume Data - HRM Traffic and Right of Way Services has obtained machine traffic counts on Spring Garden Road between Robie Street and Carlton Street periodically. The following average weekday volumes from counts obtained between 2003 and 2009 (more recent counts are not available) indicate that the volume has been about 10,000 vehicles per day (vpd) for several years:

<u>Month and Year</u>	<u>Average Weekday Volume (vpd)</u>
July 2003	9,980
July 2004	10,415
July 2005	9,885
November 2006	10,365
July 2007	9,635
August 2008	9,535
May 2009	10,655

Manual Traffic Counts - A manual traffic count, obtained during peak AM, noon, and PM periods on Friday, February 13, 2009, is tabulated in Table A-1, with peak hour volumes shown as shaded areas. The 2009 AM and PM peak hourly volumes have been increased using a 1% annual traffic volume growth rate typical of this type of urban street to provide projected 2012 AM and PM peak background volumes which are shown diagrammatically on Figure A-1, Boxes A and B and 2017 AM and PM background volumes shown diagrammatically on Figure A-1, Boxes C and D.

Trip Generation - Proposed Apartment Development - *Trip Generation, 8th Edition* (Institute of Transportation Engineers (ITE), Washington, 2008) provides trip generation rate estimates for AM and PM peak hours for various land uses. Trip generation estimates for the proposed development are included in Table 1. Since the development is expected to have much higher than average pedestrian and transit use which is typical of Spring Garden Road developments, trip generation estimates for the proposed development have been reduced by 30% as indicated in the footnotes for Table 1. It is estimated that the site will generate 25 two-way vehicle trips (7 in and 18 out) during the AM peak hour and 41 two-way vehicle trips (26 in and 15 out) during the PM peak hour.

Trip Distribution and Assignment - Since trip generation estimates have considered the higher than average pedestrian and transit use anticipated from building occupants, all trips generated by this development have been considered as primary trips. Trips exiting the site have been assigned as 30% left turns and 70% right turns to Spring Garden Road. Site generated trips (Figure A-2, Boxes A and B) have been added to the projected 2017 AM and PM peak hour volumes (Figure A-1, Boxes C and D) to provide projected 2017 intersection volumes which include site generated trips as illustrated on Figure A-2, Boxes C and D.

Land Use ¹	Number Units ²	Trip Generation Rates ¹				Trips Generated ³			
		AM Peak		PM Peak		AM Peak		PM Peak	
		In	Out	In	Out	In	Out	In	Out
High Rise Apt (Land Use 222)	110 Units	0.07	0.23	0.21	0.14	8	25	23	15
Quality Restaurant (Land Use 931)	80 Seats	0.02	0.01	0.17	0.09	2	1	14	7
Trip Generation Estimates for Proposed Development						10	26	37	22
30% Reduction for High Pedestrian and Transit Usage ⁴						3	8	11	7
Adjusted Trip Generation Estimates for Proposed Development						7	18	26	15

NOTES: 1. Trip generation rates are 'vehicles per hour per unit'. Rates are for indicated Land Uses and Land Use Codes, *Trip Generation, 8th Edition*, Institute of Transportation Engineers, 2008.
 2. Units are 'number of apartments'; 'number of seats'.
 3. Vehicles per hour for peak hours.
 4. Trip generation estimates have been reduced by 30% to account for the high pedestrian and transit usage in the Study Area adjacent to Spring Garden Road.

Intersection Performance - The level or quality of performance of an intersection in terms of traffic movement is determined by a level of service (LOS) analysis. LOS for intersections is defined in terms of delay, which is a measure of driver discomfort and frustration, fuel consumption, and increased travel time. LOS criteria (Table 2) are stated in terms of average control delay per vehicle which includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay.

LOS	LOS Description	Two Way Stop Controlled (TWSC) Intersections Control Delay (seconds per vehicle)
A	Very low delay; most vehicles do not stop (Excellent)	less than 10.0
B	Higher delay; more vehicles stop (Very Good)	between 10.0 and 15.0
C	Higher level of congestion; number of vehicles stopping is significant, although many still pass through intersection without stopping (Good)	between 15.0 and 25.0
D	Congestion becomes noticeable; vehicles must sometimes wait through more than one red light; many vehicles stop (Satisfactory)	between 25.0 and 35.0
E	Vehicles must often wait through more than one red light; considered by many agencies to be the limit of acceptable delay	between 35.0 and 50.0
F	This level is considered to be unacceptable to most drivers; occurs when arrival flow rates exceed the capacity of the intersection (Unacceptable)	greater than 50.0

The HRM *Guidelines for Preparation of Transportation Impact Studies* indicates the following critical limits for intersection evaluation:

1. the v/c ratio of an intersection exceeds 0.85;
2. the v/c ratio of an individual through movement or shared through/ turning movement exceeds 0.85;
3. the v/c ratio of an exclusive turning movement exceeds 1.0;
4. an exclusive turning movement generates queues which exceed the available turning lane storage space.

Synchro 8.0 software has been used for intersection performance evaluation of the Spring Garden Road / Carlton Street intersection. Level of service (LOS) analysis has been completed for projected 2017 AM and PM peak hour volumes without and with volumes that include site generated trips. LOS analysis results are included on Pages A-4 through A-7, Appendix A, and are summarized in Table 3.

Table 3 - Summary Level of Service Analyses - Proposed Carlton Terrace Development					
LOS Criteria	Control Delay (sec/veh), v/c Ratio, and 95% Queue (m) by Intersection Movement				Intersection LOS
	EB-LTR	WB-LTR	NB-LTR	SB-LTR	
AM Peak Hour - Projected 2017 Volumes without Site Development (Page A-4)					
Delay	0.6	0.9	14.7	12.7	2.0
v/c	0.32	0.27	0.10	0.07	-
Queue	0.4	0.6	2.8	1.7	-
AM Peak Hour - Projected 2017 Volumes With Site Development (Page A-6)					
Delay	0.7	0.9	15.2	12.9	2.3
v/c	0.32	0.27	0.11	0.10	-
Queue	0.5	0.6	2.9	2.7	-
PM Peak Hour - Projected 2017 Volumes without Site Development (Page A-5)					
Delay	0.3	0.4	18.5	15.6	1.9
v/c	0.39	0.47	0.17	0.10	-
Queue	0.3	0.3	4.8	2.7	-
PM Peak Hour - Projected 2017 Volumes With Site Development (Page A-7)					
Delay	0.9	0.4	20.2	16.6	2.4
v/c	0.41	0.48	0.19	0.15	-
Queue	0.7	0.3	5.4	4.2	-

Summary Intersection Performance Analysis - The LOS analysis indicates that the intersection provides very good levels of performance and operates well within HRM guidelines for existing traffic volumes, and will continue to do so for projected traffic volumes that include site generated trips. Site generated trips will not have any significant impact to levels of performance at the Spring Garden Road / Carlton Street intersection.

Summary and Conclusions -

1. The proposed Carlton Terrace apartment development at the north end of Carlton Street will include a 110 unit apartment building. The proposed development also includes an approximately 2,800 square foot building to be used as a restaurant abutting Spring Garden Road at the east site boundary. Construction is expected to start during 2014 with the building being completed in 2017. Site access is proposed at driveway entrances on Carlton Street.
2. The average weekday two-way volume on Spring Garden Road just west of the Carlton Street intersection has been approximately 10,000 vehicles per day (vpd) for several years.
3. Trip generation rates published in *Trip Generation, 8th Edition*, have been reduced by 30% to account for the much higher than average pedestrian and transit use which is typical of Spring Garden Road developments. It is estimated that the site will generate 25 two-way vehicle trips (7 in and 18 out) during the AM peak hour and 41 two-way vehicle trips (26 in and 15 out) during the PM peak hour.
4. Level of service analysis indicates that the Spring Garden Road / Carlton Street intersection provides very good levels of performance and operates well within HRM guidelines for existing traffic volumes, and will continue to do so for projected traffic volumes that include site generated trips.
5. Site generated trips are not expected to have any significant impact to levels of performance at the Spring Garden Road / Carlton Street intersection or to the regional street system.

If you require additional information please contact me by Email to ken.obrien@genivar.com or telephone 443-7747.

Sincerely:

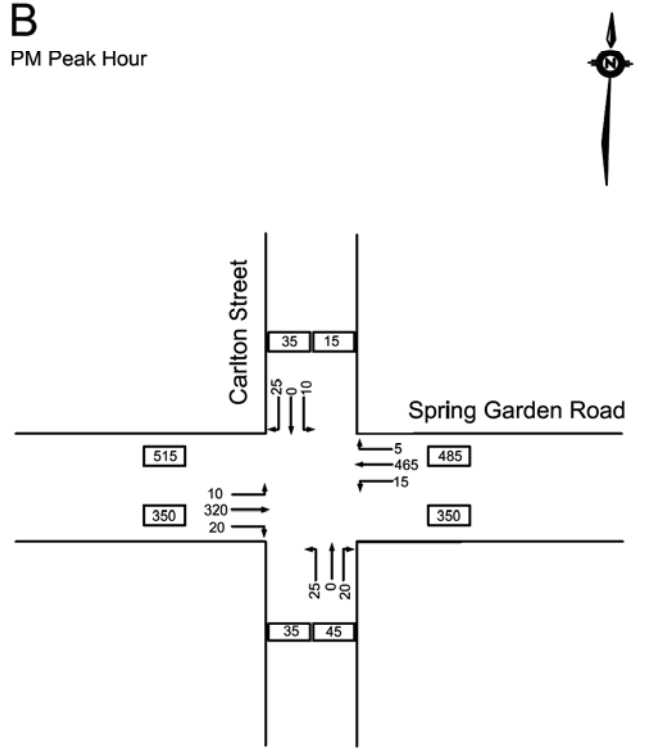
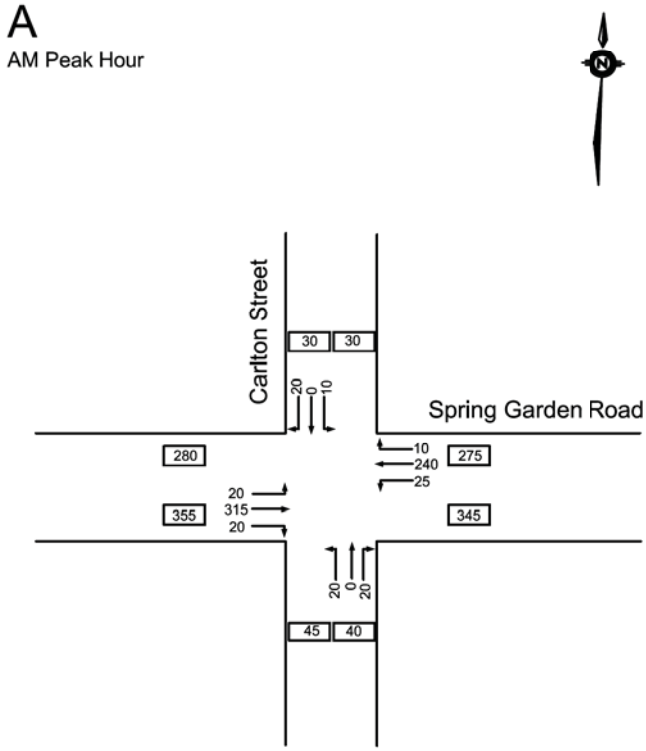
ORIGINAL SIGNED

Ken O'Brien, P. Eng.
Senior Traffic Engineer
GENIVAR Inc.



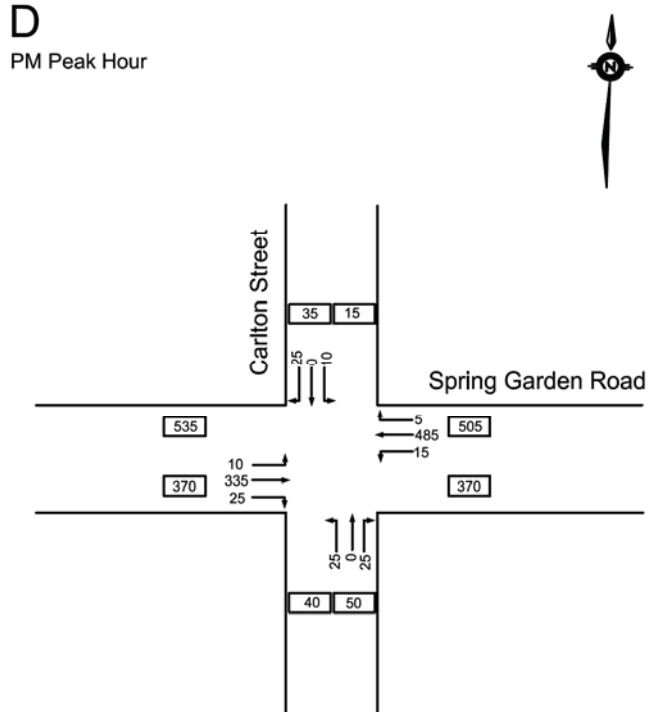
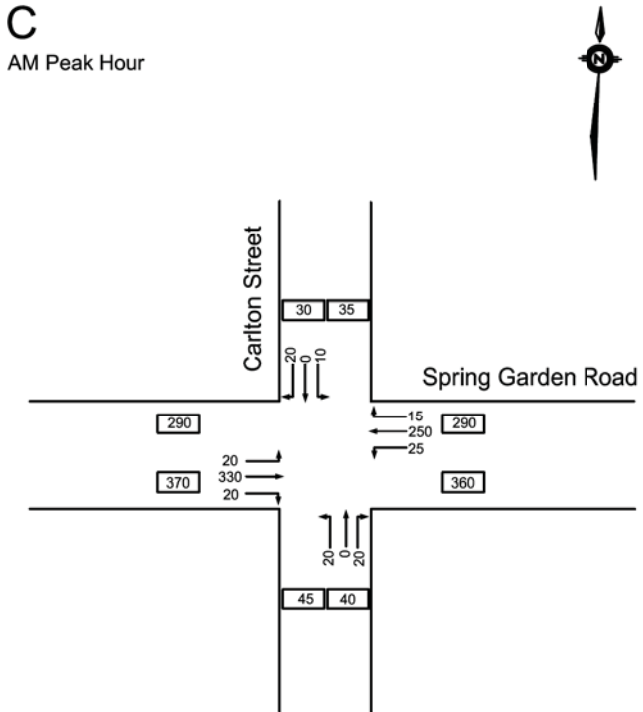
<p>Table A-1 Spring Garden Road @ Carlton Street Halifax, Nova Scotia</p> <p>Friday, February 13, 2009</p>													
Time	Carlton Street Northbound Approach			Spring Garden Road Westbound Approach			Carlton Street Southbound Approach			Spring Garden Road Eastbound Approach			Total Vehicles
	A	B	C	D	E	F	G	H	I	J	K	L	
07:30-07:45	1	0	3	3	62	3	1	0	2	4	54	5	138
07:45-08:00	2	0	2	3	56	7	0	0	6	6	62	4	148
08:00-08:15	7	0	1	5	63	2	1	1	3	4	71	3	161
08:15-08:30	6	0	6	1	61	3	2	0	6	4	85	3	177
08:30-08:45	4	0	2	5	62	4	2	0	4	5	72	8	168
08:45-09:00	0	0	7	7	42	5	2	1	4	6	83	6	163
09:00-09:15	7	1	2	9	66	0	2	0	6	3	65	3	164
09:15-09:30	4	0	7	3	59	2	4	0	5	3	54	4	145
07:30 to 08:30	16	0	12	12	242	15	4	1	17	18	272	15	624
08:30 to 09:30	15	1	18	24	229	11	10	1	19	17	274	21	640
AM Peak Hour	17	1	17	22	231	12	8	1	20	18	305	20	672
11:00-11:15	0	0	5	3	57	2	3	0	3	6	55	4	138
11:15-11:30	3	0	5	2	68	2	3	0	9	3	58	6	159
11:30-11:45	1	0	5	5	85	3	2	0	3	4	109	7	224
11:45-12:00	2	0	5	1	88	3	1	0	6	7	80	15	208
12:00-12:15	4	0	4	2	68	6	1	1	5	4	74	7	176
12:15-12:30	2	0	5	3	81	3	2	0	5	3	86	4	194
12:30-12:45	4	0	7	4	91	1	7	0	2	3	72	8	199
12:45-13:00	3	1	5	3	70	3	1	1	7	4	89	3	190
11:00 to 12:00	6	0	20	11	298	10	9	0	21	20	302	32	729
12:00 to 13:00	13	1	21	12	310	13	11	2	19	14	321	22	759
Noon Peak Hour	9	0	19	11	322	15	6	1	19	18	349	33	802
15:30-15:45	2	0	4	5	95	1	1	0	10	1	87	4	210
15:45-16:00	6	0	5	5	126	0	1	0	4	1	81	11	240
16:00-16:15	7	0	7	1	105	2	3	0	9	4	66	4	208
16:15-16:30	8	0	4	4	99	2	1	0	3	3	77	4	205
16:30-16:45	4	0	5	5	120	0	5	0	8	2	87	2	238
16:45-17:00	3	0	7	5	95	3	2	1	5	1	83	5	210
17:00-17:15	6	1	3	2	75	1	1	2	2	1	79	5	178
17:15-17:30	12	0	4	3	110	2	0	0	3	4	73	5	216
15:30 to 16:30	23	0	20	15	425	5	6	0	26	9	311	23	863
16:30 to 17:30	25	1	19	15	400	6	8	3	18	8	322	17	842
PM Peak Hour	25	0	21	15	450	4	10	0	24	10	311	21	891

2012 AM and PM Peak Hour Background Volumes



NOT TO SCALE

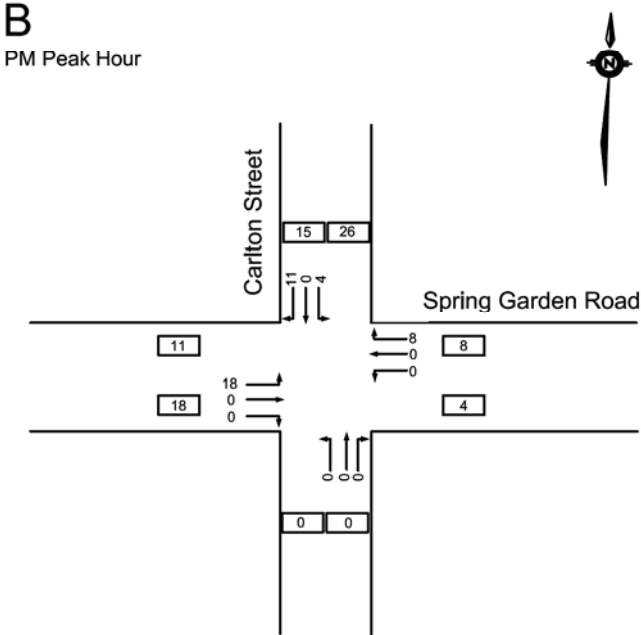
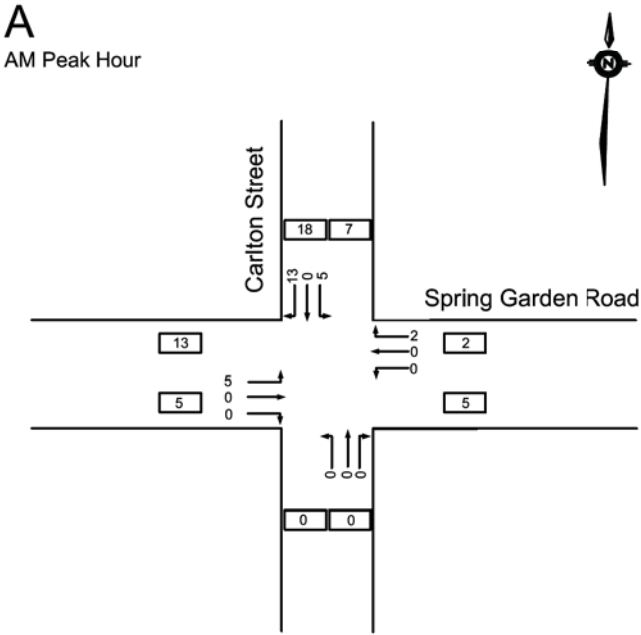
2017 AM and PM Peak Hour Background Volumes



NOT TO SCALE

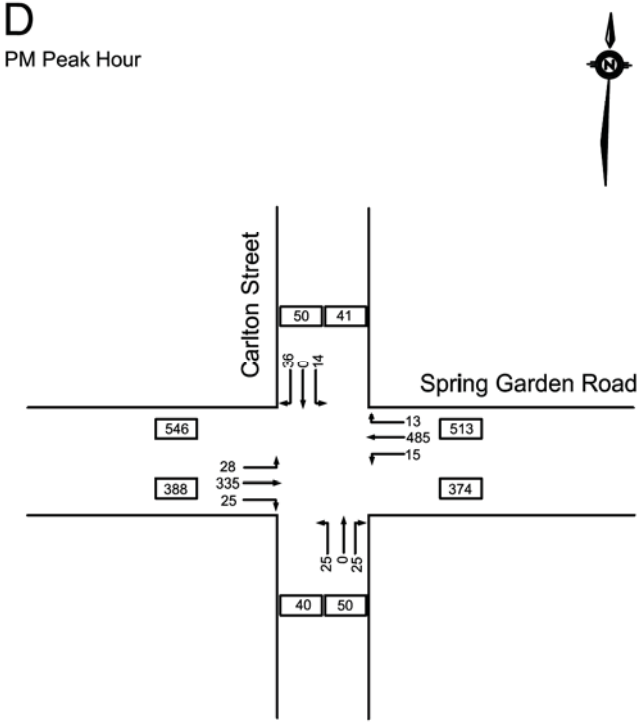
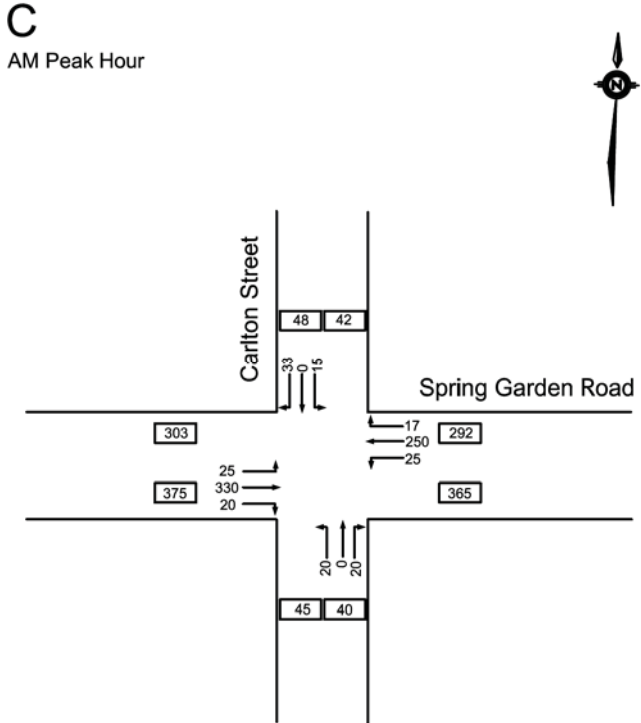


Estimated Site Generated Trips



NOT TO SCALE

2017 AM and PM Peak Hour Volumes with Site Generated Trips



















NOT TO SCALE



1: Carlton Street & Spring Garden Road

2017 AM Peak without Site Development (Fig A1-C)

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (veh/h)	20	330	20	25	250	15	20	0	20	10	0	20
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	22	359	22	27	272	16	22	0	22	11	0	22
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	288			380			769	755	370	769	758	280
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	288			380			769	755	370	769	758	280
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	98			98			93	100	97	96	100	97
cM capacity (veh/h)	1274			1178			300	324	676	298	323	759
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	402	315	43	33								
Volume Left	22	27	22	11								
Volume Right	22	16	22	22								
cSH	1274	1178	415	501								
Volume to Capacity	0.02	0.02	0.10	0.07								
Queue Length 95th (m)	0.4	0.6	2.8	1.7								
Control Delay (s)	0.6	0.9	14.7	12.7								
Lane LOS	A	A	B	B								
Approach Delay (s)	0.6	0.9	14.7	12.7								
Approach LOS			B	B								
Intersection Summary												
Average Delay			2.0									
Intersection Capacity Utilization			33.7%		ICU Level of Service				A			
Analysis Period (min)			15									

1: Carlton Street & Spring Garden Road

2017 PM Peak without Site Development (Fig A1-D)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (veh/h)	10	335	25	15	485	5	25	0	25	10	0	25
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	11	364	27	16	527	5	27	0	27	11	0	27
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	533			391			989	965	378	989	976	530
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	533			391			989	965	378	989	976	530
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	99			99			87	100	96	95	100	95
cM capacity (veh/h)	1035			1167			211	249	669	213	245	549
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	402	549	54	38								
Volume Left	11	16	27	11								
Volume Right	27	5	27	27								
cSH	1035	1167	320	378								
Volume to Capacity	0.01	0.01	0.17	0.10								
Queue Length 95th (m)	0.3	0.3	4.8	2.7								
Control Delay (s)	0.3	0.4	18.5	15.6								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.3	0.4	18.5	15.6								
Approach LOS			C	C								
Intersection Summary												
Average Delay			1.9									
Intersection Capacity Utilization			44.3%		ICU Level of Service				A			
Analysis Period (min)			15									

















1: Carlton Street & Spring Garden Road

2017 AM Peak with Site Development (Fig A2-C)

Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (veh/h)	25	330	20	25	250	17	20	0	20	15	0	33
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	27	359	22	27	272	18	22	0	22	16	0	36
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	290			380			795	768	370	781	770	281
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	290			380			795	768	370	781	770	281
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	98			98			92	100	97	94	100	95
cM capacity (veh/h)	1272			1178			281	317	676	292	317	758
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	408	317	43	52								
Volume Left	27	27	22	16								
Volume Right	22	18	22	36								
cSH	1272	1178	397	506								
Volume to Capacity	0.02	0.02	0.11	0.10								
Queue Length 95th (m)	0.5	0.6	2.9	2.7								
Control Delay (s)	0.7	0.9	15.2	12.9								
Lane LOS	A	A	C	B								
Approach Delay (s)	0.7	0.9	15.2	12.9								
Approach LOS			C	B								
Intersection Summary												
Average Delay			2.3									
Intersection Capacity Utilization			35.2%		ICU Level of Service				A			
Analysis Period (min)			15									

1: Carlton Street & Spring Garden Road

2017 PM Peak with Site Development (Fig A2-D)

												
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Volume (veh/h)	28	335	25	15	485	13	25	0	25	14	0	36
Sign Control		Free			Free			Stop			Stop	
Grade		0%			0%			0%			0%	
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Hourly flow rate (vph)	30	364	27	16	527	14	27	0	27	15	0	39
Pedestrians												
Lane Width (m)												
Walking Speed (m/s)												
Percent Blockage												
Right turn flare (veh)												
Median type		None			None							
Median storage (veh)												
Upstream signal (m)												
pX, platoon unblocked												
vC, conflicting volume	541			391			1045	1012	378	1033	1019	534
vC1, stage 1 conf vol												
vC2, stage 2 conf vol												
vCu, unblocked vol	541			391			1045	1012	378	1033	1019	534
tC, single (s)	4.1			4.1			7.1	6.5	6.2	7.1	6.5	6.2
tC, 2 stage (s)												
tF (s)	2.2			2.2			3.5	4.0	3.3	3.5	4.0	3.3
p0 queue free %	97			99			85	100	96	92	100	93
cM capacity (veh/h)	1027			1167			186	229	669	196	227	546
Direction, Lane #	EB 1	WB 1	NB 1	SB 1								
Volume Total	422	558	54	54								
Volume Left	30	16	27	15								
Volume Right	27	14	27	39								
cSH	1027	1167	291	364								
Volume to Capacity	0.03	0.01	0.19	0.15								
Queue Length 95th (m)	0.7	0.3	5.4	4.2								
Control Delay (s)	0.9	0.4	20.2	16.6								
Lane LOS	A	A	C	C								
Approach Delay (s)	0.9	0.4	20.2	16.6								
Approach LOS			C	C								
Intersection Summary												
Average Delay			2.4									
Intersection Capacity Utilization			43.8%		ICU Level of Service				A			
Analysis Period (min)			15									

**ATTACHMENT H – EXCERPT FROM THE
HALIFAX PENINSULA LAND USE BY-LAW**

R-3 ZONE

MULTIPLE DWELLING ZONE

44(1) The following uses shall be permitted in any R-3 Zone:

- (a) R-1 and R-2 and R-2T uses;
- (aa) R-2A uses in "**South End Area**";
- (b) boarding house;
- (c) lodging or rooming house;
- (d) apartment house;
- (e) uses accessory to any of the foregoing uses, if not specifically prohibited;

OTHER USES:

- (f) in any one building one office for rendering professional or personal services, provided that the net area for such purposes does not exceed 700 square feet;
- (g) special care home;
- (h) greenhouse;
- (i) the office of a Consulate located in a single family dwelling provided such dwelling is used by the Consulate as his private residence;
- (ia) **day care facility; (RC-Mar 3/09;E-Mar 21/09)**
- (ib) parking lots and parking structures existing on the date of adoption of this Section, in Area 8 of the "**Peninsula North Area**".

COMMERCIAL USES:

One each of the following uses only if located in an apartment house which contains not less than one hundred self-contained dwelling units and located in "**Schedule B**";

- (j) retail foodstuff store of not more than 600 square feet;
- (k) restaurant of not more than 900 square feet of dining area, exclusive of kitchen, storage, washroom, staff areas, and the like;

One each of the following uses only if located in an apartment house which contains not less than one hundred self-contained dwelling units and which is erected on a lot of more than one acre:

- (l) barber shop;
- (m) beauty parlour;
- (n) dry cleaning distribution station;

- (o) valet service;
- (p) restaurant;
- (q) florist shop;
- (r) newsstand;
- (s) health club;
- (t) coin-operated vending machines;
- (u) retail foodstuff store of not more than 600 square feet;

Provided that:

- (i) these uses shall be for the exclusive use of the residents of such apartment house or their guests;
- (ii) there shall be no advertising or identification of the uses on the outside of the building;
- (iii) there shall be no visible indication from the exterior of the building that the commercial uses described in this section are carried on;
- (iv) there shall be no direct access from the exterior of the building to any of the commercial uses described in this section other than emergency access places in case of fire.

44(2) No person shall in any R-3 Zone carry out, or cause or permit to be carried out, any development for any purpose other than one or more of the uses set out in subsection (1).

44(3) No person shall in any R-3 Zone use or permit to be used any land or building in whole or in part for any purpose other than one or more of the uses set out in subsection (1).

DISPLAY WINDOW

44(4) Where any building is used in an R-3 Zone for any of the purposes described in clause (f) of subsection (1) of this section, no display window shall be permitted, nor shall any evidence of the use of such building for such purposes be visible from the exterior of such building; PROVIDED, however, that nothing contained herein shall prohibit the display of a sign not exceeding one square foot in area, which bears the name and profession as set forth in clause (f) of subsection (1) of this section, of any person occupying such building.

SOUTH END

44A Notwithstanding the provisions of subsections 44(1) and 44(4), an apartment house in the "**South End Area**", and which contains 100 or more self-contained dwelling units may include those commercial uses which are permitted in Section 59A(1), except a bowling alley, a motion picture theatre and a service station, provided that such uses are located on the ground floor of the apartment house and are separately accessible from the building exterior.

SIGNS/ADVERTISING

- 44B Exterior advertising of the commercial uses described in Section 44A shall be permitted, provided that such advertising is non-illuminated.

UNIT MIX

- 44C An apartment house in the "**South End Area**" shall, of the total amount of dwelling units, be required to include at least one dwelling unit of a minimum of 800 square feet in floor area for every three dwelling units, each of which is less than 800 square feet in floor area.

- 44D An apartment house in the "**Peninsula Centre Area**" area shall, of the total amount of dwelling units, be required to include at least one dwelling unit of a minimum of 800 square feet in floor area for every two dwelling units, each of which is less than 800 square feet.

SPECIAL PARKING

- 44E Notwithstanding any other provision of this by-law, an apartment house in the "**South End**" and "**Peninsula Centre Areas**", shall be required to provide one parking space for each dwelling unit which is 800 square feet or greater, and one parking space for every two dwelling units, each of which is less than 800 square feet.

- 44F A lot which abuts a street at more than one location or which abuts two or more streets shall not be used for R-3 uses in the "**South End Area**", except for corner lots which abut at least two streets on a continuous uninterrupted line. For greater certainty, a corner lot may be subdivided and developed for R-3 uses in accordance with the requirements of this by-law, notwithstanding that a through lot may be created.

R-1, R-2, AND R-2T USES IN R-3 ZONE

- 45 Buildings erected, altered, or used for R-1, R-2 and R-2T uses in an R-3 Zone shall comply with the requirements of their respective zones.
- 45(2) Buildings erected, altered or used for R-2A uses in an R-3 Zone in the "**South End Area**" shall comply with the requirements of the R-2A Zone with the exception of Sections 43AD (vii) and viii), 43AE and 43AG.

SIGNS

- 46(1) No person shall erect or display any billboard or illuminated sign in an R-3 Zone.
- 46(2) A non-illuminated sign may be erected in an R-3 Zone if, in the opinion of the

Inspector of Buildings, such sign is of reasonable proportion and will not constitute a hazard to the public or a nuisance to the owners of the property in the area.

- 47 Where any building is erected or altered or used for R-3 uses in an R-3 Zone, such building shall comply with the following requirements:

MINIMUM LOT AREA

- 47(1) (a) The minimum lot area upon which such building is located shall be 8,100 square feet with a minimum continuous street frontage of at least 90 feet on one street;
- (b) (Deleted)

DISTANCE FROM LOT LINE - 80° ANGLE

- 47(2) (a) The distance from any part of such building and any official street line or lines abutting upon such lot shall be no less than 20 feet measured at right angles to any such official street line or lines; provided, however, that such distance may be reduced to not less than 10 feet measured at right angles to any such official street line or lines if that part of the building which is less than 20 feet from any such official street line or lines is entirely contained within the arms of an 80 degree horizontal angle as determined in subsection (3) of this section.
- (b) The distance from any part of such building and any lot line of such lot other than an official street line shall be not less than 10 feet measured at right angles to such lot line.
- (c) All windows and doors serving habitable rooms in such building shall be located not less than 10 feet from any lot line of such lot measured at right angles to such lot line.
- (d) Notwithstanding the provisions of clauses (a) and (b) of this subsection, the distance from any part of such building, not containing any windows or doors serving habitable rooms, to any official street line or lot line, may be less than the distance prescribed in said clauses (a) and (b) or may extend to any such official street line or lot line of the lot upon which such building is located, provided that:
- (i) the height of such part of the building does not exceed 5 feet above the natural ground level measured at any point on any official street line abutting such lot and extending for a horizontal distance of 10 feet measured at right angles to any such official street line;
 - (ii) the height of such part of the building does not exceed 5 feet above the natural ground level measured at any point on any lot line of such lot other than an official street line; and
 - (iii) the building is so designed that it does not interfere with traffic safety.
- (e) (Deleted)

SIZE OF BUILDING - 60 ANGLE

- 47(3)
- (a) Subject to the provisions of subsection (2), such building or any part thereof shall not project beyond the angular planes determined by constructing such angular planes over such lot
 - (i) from each lot line at natural or finished ground level, whichever is the lower, at a vertical angle of 60 degrees above the horizontal and measured perpendicular to such lot line or, in the case of a curved lot line, perpendicular to the tangents of all points of such lot line; provided, however, that where the natural ground at the lot line is more than 5 feet above the finished ground level established at any point on the wall opposite the lot line and where the horizontal distance to the face of any part of such wall or its vertical projection is less than 50 feet, the angular planes shall be constructed over the lot from all points on the intersections of the vertical projections of the lot line and the horizontal projection of the finished ground level; or
 - (ii) in the case where a lot line of such lot coincides with an official street line, from the center line of such street or from any intervening line parallel to such center line; provided, however, that
 - (A) the distance from the line on which the plane is constructed and the lot line does not exceed 30 feet; and
 - (B) the vertical angle of 60 degrees is constructed perpendicular to the line on which the plane is constructed or, in the case of a curved line, perpendicular to the tangents of all points of the curved line.
 - (b) Notwithstanding the provisions of clause (a) of this subsection and subject to the provisions of subsection (2), any part of such building may project beyond any prescribed 60 degree angular plane if:
 - (i) the projection through the plane subtends a horizontal angle not exceeding 80 degrees formed by lines drawn from a point on the line on which the 60 degree angular plane is constructed opposite to the center of the projection; and
 - (ii) the extremities of the projection are enclosed by the arms of such 80 degree horizontal angle.

DISTANCE BETWEEN EXTERNAL WALLS - 85 ANGLE

- 47(4)
- (a) For the purposes of this subsection:
 - (i) "base line" means, in the case of a wall rising from the ground, the natural or finished level of the ground adjoining the base of the wall, whichever is lower, and in all other cases means the lowest line of the wall above the natural or finished level of the ground, whichever is lower;

- (ii) a wall supported by construction above posts, pillars, or other open construction shall be deemed to rise from the ground and the base line of the wall shall be deemed to be the line on which the projection downward of the face of the wall meets the natural or finished level of the ground, whichever is lower;
- (iii) where external walls are not parallel to each other but the angle of divergence does not exceed 85 degrees, such walls shall be deemed to face each other.

65 ANGLE

- (b) The provisions of this subsection (4) shall only apply if any part of such building is erected within the arms of horizontal angles of 65 degrees constructed outwards at the natural level of the ground, from the nearest extremities of external walls that face each other; provided, however, that where the two extremities of one such wall are, respectively, equidistant from the opposite extremities of the other wall or where the two extremities of one wall are equidistant from the nearest extremity of the other wall, the 65 degree horizontal angle may be constructed from either pair of equidistant extremities.

40 ANGLE

- (c) The distance between any external walls of such building that face each other shall be not less than 50 feet, and any part of such building shall not project beyond any of the angular planes determined by constructing such angular planes outwards from the base line of each such external facing wall of each part of such building at a vertical angle of 40 degrees above the horizontal and measured perpendicular to such base line or, in the case of a curved base line, perpendicular to the tangents of all points of such curved base line.
- (d) Where two external walls of such building face each other and neither wall contains any door or window serving a habitable room, the provisions of clause (c) of this subsection shall not apply but the distance between such walls shall not be less than six feet.

BALCONIES, CORNICES, EAVES, AND CANOPIES

47(5) Notwithstanding the provisions of subsections (3) and (4) of this section, separate individual balconies, which are open on three sides, cornices, eaves, and canopies may project through the angular planes as determined in such subsections; provided, however, that any part of such projection shall be not less than 10 feet from any lot line of such lot.

48 Where any building is erected, altered, or used as a **boarding house, lodging or rooming house, an apartment house or special care home** in an R-3 Zone, such

building, in addition to the requirements hereinbefore set out in Section 47, shall comply with the following requirements:

POPULATION DENSITY

48(1) The population density of such building shall not exceed:

- (a) 250 persons per acre if located in "**Schedule A**"; and
- (b) 125 persons per acre if located in an area other than that described in Schedule "A".

OPEN SPACE

- 48(2)
- (a) The lot upon which such building is located shall contain a minimum of:
 - (i) 120 square feet of open space for each person occupying such building in a dwelling unit containing two or more bedrooms, of which at least 100 square feet shall be landscaped open space; and
 - (ii) 80 square feet of open space for each person residing within such building in a dwelling unit containing one bedroom, of which at least 70 square feet shall be landscaped open space; and
 - (iii) 50 square feet of landscaped open space for each person residing within such building in a bachelor unit if located in "**Schedule B**"; and
 - (iv) 80 square feet of open space, of which at least 70 square feet shall be landscaped open space, for each person residing within such building in a bachelor unit if located within an area other than "**Schedule B**".
 - (b) The occupancy of such building shall be calculated on the basis of one person for each habitable room contained therein.
 - (c) For the purpose of this subsection, the roof, or any portion thereof, of any part of such building that has no residential accommodation included below such roof or portion thereof may be calculated as landscaped open space; provided that:
 - (i) no part of such roof is more than 5 feet above the ground level of at least one lot line of such lot; and
 - (ii) such roof, or portion thereof, is capable of being used as landscaped open space.
 - (d) **Notwithstanding the provisions of Subsection 48(2)(c) above, a maximum of 40% of the landscaped open space requirement for dwelling units containing two or more bedrooms may be transferred to the building rooftop, including rooftops greater than 5 feet above the ground level; provided that:**
 - (i) **the rooftop landscaped open space is contiguous and not less than 600 square feet (56 m²) in area; and**
 - (ii) **the rooftop landscaped open space is fully accessible for the common use of the occupants of the building. (PCC-Apr**

14/08;E-May 5/08)

OPEN SPACE FOR SPECIAL CARE HOME

48A

A minimum of 35 percent of the lot area of any lot on which a building is erected, altered or used as a special care home, shall consist of landscaped open space.

ATTACHMENT I – APPLICABLE POLICIES FROM THE MUNICIPAL PLANNING STRATEGY FOR HALIFAX

SECTION VI – PENINSULA CENTRE AREA PLAN

1. RESIDENTIAL ENVIRONMENTS

Objective: The maintenance of Peninsula Centre as a predominantly low-rise residential neighbourhood with an emphasis on housing accommodation for family households.

1.1 In the Peninsula Centre Area, residential development shall occur through retention and rehabilitation of housing stock, and provisions shall be made for infill and, in selected areas, redevelopment.

1.1.1 The City shall encourage the retention and creation of dwelling units suitable for families with children.

1.1.2 For the purposes of this Plan, a single definition of infill housing shall not be employed. The diverse physical and social elements of residential areas should be respected through the selective application of several forms of compatible infill housing.

1.1.3 The forms of infill housing permitted in Peninsula Centre shall include:

- (a) interior conversion;
- (b) additions to existing structures;
- (c) filling-in-between existing buildings; and
- (d) building on vacant lots.

1.1.4 For the purposes of this Plan, the concept of compatibility shall be deemed to require that infill housing projects are compatible with and enhance the existing development context of a neighbourhood. The City shall use as a guideline in considering rezonings, zoning amendments or contract agreements the key principle of not significantly changing the character of an area when reviewing infill housing proposals.

1.1.5 Without limiting the generality of Policy 1.1.4 above, the City shall, in reviewing proposals for compatibility with the surrounding area, have regard for the relationship of the proposal to the area in terms of the following:

- (a) land use;
- (b) scale and height;
- (c) population density;
- (d) lot size, lot frontage, setback, lot coverage and open space; and
- (e) service requirements, including parking.

1.1.6 Further to Policy 1.1.5 above, existing development standards will be assessed against their capacity to achieve the policies of this Detailed Area Plan with respect to infill housing and with respect to preservation of existing housing. Existing development standards will be amended as necessary to implement the policies of this Plan.

1.1.7 Further to Policy 1.1.6 above, open space and landscaping will be given special attention to ensure that amenity space in new development projects is useable and to foster attractive residential environments which address the needs of a variety of household types.

1.1.8 The Zoning By-law shall be amended to include height limitations for development in accordance with Policies 4.6.3, 4.7.2, 8.1.1 and 8.4.1 and in accordance with the general intent for land-use control as defined by the policies of this Plan. Where there is not specific guidance by the policies of this Plan for specific height limitations, such limitations shall be included based on:

- (i) the forms of development and distribution of land use identified on the Generalized Future Land Use Map (Map 2) of this Plan;
- (ii) the necessity to ensure that appropriate development of any given lot may be secured within the policies of this Plan; and
- (iii) to fulfill the policy intent that quality residential, commercial, institutional and industrial environments are maintained and encouraged without undue impact on adjacent land uses.

1.2 The City shall encourage the retention and creation of family-type housing in Peninsula Centre.

1.2.1 Family-type housing units should be provided with private open space at grade comprising both soft-surfaced and hard-surfaced areas for the exclusive use of occupants of the building in which said family units are located.

1.2.2 Any new residential development containing more than twenty family-type dwelling units should provide a children's play area enclosed on all four sides, landscaped and buffered appropriately, of not less than 2,000 square feet. Such play areas shall be required to be located on the south or west side of the building and be located in such a manner as to be visible from the building.

- 1.2.3 In reviewing applications for rezonings, zoning amendments, or contract agreements, the City shall be guided by Policies 1.2.4 and 1.2.5 with respect to family-type housing units.
- 1.2.4 Residential development shall be planned to ensure maximum buffering between children's activity areas and parking areas, streets and other similar safety hazards.
- 1.2.5 For development applications which include family-type housing units, the City shall have regard for the provision of opportunities for visual surveillance and supervision of children's play areas through site designs which maximize the views from windows in the building and from public areas to children's activity areas.
- 1.3 For the purposes of this Plan, the City shall further define residential environments as comprising three categories:
- (i) low-density residential;
 - (ii) medium-density residential; and
 - (iii) high-density residential.
- 1.6 Buildings in areas shown as high-density residential on the Future Land Use Map of this Plan shall be required to provide, at a minimum, a mix of family and non-family type dwelling units at a ratio of 1:2.
- 1.7 Notwithstanding the policies above, where a residential building is proposed to contain a maximum of two units, only one unit shall be required to be a family-type unit.
- 1.11 The City shall pay particular attention in developing appropriate zoning regulations to carry out the policies of this Plan, and in reviewing applications for rezonings, zoning amendments or contract agreements for areas of transition between residential and non-residential uses, to the relationship between such uses, and shall attempt to minimize any negative impacts which may potentially occur.

2. COMMERCIAL FACILITIES

Objective: The provision for a variety of neighbourhood convenience stores and minor commercial uses in convenient and accessible locations which do not adversely affect adjacent residential uses.

- 2.2.1 In areas east of Robie Street and north of Spring Garden Road designated as high-density residential or in areas designated as residential-commercial mix on the Future Land Use Map of this Plan, the City may permit the ground floor of buildings to be occupied by office uses, in accordance with Policy 8.1.3 of this Plan.

- 2.2.2 The City shall deny rezonings to permit a minor commercial use in areas not designated as commercial on the Future Land Use Map of this Plan.
- 2.3 Commercial uses of a City-wide or regional nature shall not be permitted in Peninsula Centre in accordance with Part II, Section II, Policies 3.2 and 3.2.1 of the Municipal Development Plan.

8. SUB-AREA POLICIES

Sub-Areas

Definition: For the purpose of setting out the detailed sub-area policies of this Plan, sub-areas shall be identified as shown on Map 1.

8.1 SPRING GARDEN ROAD SUB-AREA

- 8.1.1 The City shall amend its zoning by-laws to include a height restriction on development in the vicinity of the Public Gardens so as to ensure a minimum of shadow casting on the Public Gardens.
- 8.1.2 The City shall consider an application under the provisions of Section 33(2)(b) of the Planning Act for a development in the Spring Garden Road Sub-Area north of Spring Garden Road which would exceed the height precinct so established through Policy 8.1.1 above, and, in so doing, the City shall require that any proposed development not cast a significant amount of shadow on the Public Gardens during that period of the year during which the Public Gardens is open to the public.
- 8.1.3 The City shall consider an application under the provisions of Section 33(2)(b) of the Planning Act for inclusion of office uses in an apartment building, provided that:
- (i) the said uses are located on the ground floor of the building;
 - (ii) access is separately accessible to the said use; and
 - (iii) parking spaces associated with said uses are separately accessible for spaces associated with the apartment uses.
- 8.1.4 For the area designated "High-Density Residential" on the southwest intersection of Spring Garden Road and Summer Street and extending to College Street, the City may consider applications for residential developments under the development agreement provisions of the Planning Act beyond the height precincts established pursuant to Policy 8.1.1, provided that no development shall be permitted which would cast shadows on the Public Gardens any day between February 21 and October 21 each year.

IMPLEMENTATION POLICIES

- 3.11 Further to Policies 1.8, 1.12, 6.1.1, 8.1.2, 8.1.3, 8.1.4, and 8.3.3 respectively in Section VI of this Plan, the City may, under the development agreement provisions of the Planning Act, issue a development permit for a development which would not otherwise meet the provisions of the Land Use By-law.
- 3.11.1 In entering agreements pursuant to Policy 3.11, Council shall be guided by the policies contained in Section VI of this Plan, and shall not enter into agreements which are inconsistent with those policies of this Plan.

**ATTACHMENT J
PUBLIC INFORMATION MEETING MINUTES**

HALIFAX REGIONAL MUNICIPALITY

Public Information Meeting

Case No. 18270 – Development Agreement for 5885 Spring Garden Road

Wednesday, September 11, 2013

7:00 p.m.

Dalhousie University Dentistry Building

STAFF IN

ATTENDANCE:

Jillian MacLellan, Planner, Planning Applications
Alden Thurston, Planning Technician, Planning Applications
Nancy Bellefontaine, Planning Controller, Planning Applications

ALSO IN

ATTENDANCE:

Councillor Waye Mason, District 7
Jeffrey Haggett, Genivar Consulting, Applicant
Jeff Barss, Killam Properties, Property Owners
Dan Sampson, Killam Properties, Property Owners
James Bugden, Killam Properties, Property Owners
Mike MacLean, Killam Properties, Property Owners
Robert Richardson, Killam Properties, Property Owners

PUBLIC IN

ATTENDANCE:

Approximately 110

1. Call to order, purpose of meeting – Jillian MacLellan

The public information meeting (PIM) was called to order at approximately 7:20 p.m. at the Dalhousie University Dentistry Building. Ms. MacLellan introduced her colleagues, Nancy Bellefontaine and Alden Thurston; Councillor Waye Mason, District 7; and the applicant, Jeffrey Haggett, Genivar Consulting and represented the property owners, Killam Properties.

The purpose of the meeting was to identify that HRM has received an application, explain the proposal and planning process involved, and receive feedback, comments and questions from members of the public.

Ms. MacLellan explained that no decisions will be made at this meeting and advised that at any time throughout the application process she can be contacted with any question, comments or concerns the public may have. It was also noted that before Council can make a decision regarding this application, a public hearing is required.

2. Overview of planning process – Jillian MacLellan

Ms. MacLellan provided an overview of the planning process. Upon receiving a planning application it is reviewed by staff to ensure it is complete. The PIM is the first step in the application process in order to obtain feedback from the public at the early stages. HRM will then have an internal staff review where the application will be reviewed by various departments including HRM traffic engineers and we will also review the impact to the Public Gardens. Once the public feedback is reviewed, and staff reviews the application, a Development Agreement (DA) will be drafted with consultation from with the applicant and other municipal departments. Once the DA is complete, staff will prepare a staff report that will go to Halifax and West Community Council (HWCC) which will provide a recommendation as to whether or not we feel the application meets the intent policies. Before Council can make a decision regarding the application a Public Hearing (PH) is required and this opportunity will provide another formal setting for concerned residents to provide feedback on the application. Ms. MacLellan advised that PH requires notification and that if those in attendance received notification regarding this meeting, they will also receive notification regarding the PH. However Ms. MacLellan expressed the importance of signing the sign-up sheet at this meeting to ensure everyone receives future notifications regarding this application. After the PH, and once Council has rendered a decision regarding the agreement, there is a 14 day appeal process through the Nova Scotia Utility and Review Board for either the applicant or members of the public to appeal that decision.

3. Presentation of Proposal – Jillian MacLellan

The PIM is for Case No. 18270 which relates to a proposal, from Genivar, for a 20 storey addition to the rear of the property located at 5885 Spring Garden Road and also includes a single storey commercial addition along Spring Garden Road. The property sits on the corner of Spring Garden Road and Carlton Street, Halifax.

The site is currently developed with an 11 storey building containing 212 units along with a dentistry office located on the ground floor and was constructed in 1962. Ms. MacLellan explained that this area is made up of a variety of different types of uses. There are many commercial uses along Spring Garden Road that range from coffee shops/restaurants, to office uses and drug stores, along with many residential type uses. It was noted that the property is in close proximity to some major public open spaces such as Camp Hill Cemetery and just north of the property is the Public Gardens, Victoria Park which are both east of the property. The subject property is also close to various heritage properties. Many views of the existing/proposed property and parking were shown.

Ms. MacLellan reviewed the planning background concerning this property advising that it is located in the Halifax Plan Area and regulated through the Halifax Peninsula Land Use By-law and zoned R-3. The property is designated High Density Residential in the Peninsula Centre Secondary Plan and within that secondary plan it is located in the Spring Garden Road Sub Area. It is further located within Schedule A and B of the LUB.

Ms. MacLellan explained that the R-3 Zone (Multiple Unit Residential) does allow for multi-unit dwellings. However, because the property is located in Schedule A it does have a residential density limit of 250 persons per acre and since it is also located in Schedule B it is permitted to have commercial uses on the ground floor which are to be associated mainly with the residential zone.

Ms. MacLellan explained that in most cases within HRM height is regulated through the zone. In this area there is a height precinct of 45' which is measured from the grade to the top of the floor. This was shown as a guide of what could be done as of right. Ms. MacLellan explained that there is policy within the Spring Garden Road Sub Area that allows the consideration of buildings that exceed the 45' height limitation through a DA. One key when looking at a DA for such an application is how the proposed development will impact the Public Gardens and to assess shadow impact. Ms. MacLellan further explained, that when reviewing such applications the MPS allows for further consideration of varying other requirements of the LUB.

Ms. MacLellan reviewed slides showing the property indicating the locations of the new development. The slides displayed the proposed additions to the property and access to the new development and explained that all parking would then be underground. Ms. MacLellan also revealed a proposed rendering of the property with a view from Spring Garden Road and explained that the applicant will be presenting further views of the proposed building within their presentation.

Presentation of Proposal – Jeffrey Haggett, Genivar Consulting

Jeffrey Haggett, Genivar Consulting, introduced himself and explained that he is representing Killam Properties and the history of his relationship with them. Mr. Haggett explained that Killam Properties is committed to the core of downtown Halifax and are committed to delivering a high quality building in a neighborhood that is already high quality and hoping to build a better community for the long term.

There are 6 key elements that were looked at for sustainability for this project. Killam Properties does not believe that just building more buildings and bringing more people to the downtown core is the only solution. They recognize the need to build places that complement the neighborhood and from the inside out. This project, Carlton Terraces, proposes a high quality living standard that will not only be experienced by new residents but also by neighbors and the every day passer by. Spring Garden Terrace is a 50 year old building located on the property. Carlton Terrace, the proposed addition, will be located to the rear of the Spring Garden Terrace which is currently used for surface parking. Further the project includes a proposed commercial addition on Spring Garden Road.

Some interesting history to take into account is that originally this building was conceived as an "I" shaped building. It would have had an enclosed courtyard and it would have emulated the same kind of addition and the shape of the front of the building would have been repeated along the back of the building stretching along Camphill Cemetery. One of the key elements being committed to is engaging and listening to the public. In March 2010 Genivar/Killam Properties met with the community, and introduced the project idea. Mr. Haggett displayed some inspirations of the workshop held where the people were asked to build the kind of building they could see on that lot. The principal reason for this type of exercise was not so much for fun, but to actually engage people and encourage them to express the principals that were important to them to be considered during the design process. Many ideas were heard and Mr. Haggett reviewed the following key elements.

It is understood from the event that the people want various design features to be incorporated into the building including those to maintain views and access to light as much as possible; have Carlton Terrace be as close to Carlton Street as possible and consider terracing towards Garden Crest; utilizing green building design; and were interested in how environmentally sensitive the project will be. The developer understands that people in both of the adjacent buildings would

like to see an update to Spring Garden Terrace and of course there were questions about construction period and noise.

Mr. Haggett advised his client, Killam Properties, had an application ready November 2011 but was requested by Halifax Regional Municipality (HRM) to postpone submitting that application due to the Corridor Study. The Corridor Study is ongoing; Phase 1 is almost complete and Phase 2 will be starting up after the Regional Plan is adopted which will hopefully be some time this year. Mr. Haggett expressed that his client was thrilled to see, as they met with people through the engagement and listened to them, the majority were comfortable with adding additional density, or extra people, living in this particular corridor.

Through consultation with the public the applicant was able to incorporate many features into the building design. One of the key elements heard was placing the building back in the corner and away from Garden Crest, away from the existing Spring Garden Terrace, and that is what is shown in the design. There will be 2m and 3m setbacks on the podium to help diminish the impact visually to the Camphill Cemetery and also to Carlton Street.

Tower separation is something the public deemed also important. Presently Spring Garden Terrace to Summer Crest is 58.7' distance. The distance between Summer Crest and Garden Crest is 80' approximately as Killam wanted to ensure the public knew they heard their request to match that distance which they are pleased not only to match, but passed it at an 88' distance between Garden Crest and the proposed Carlton Terrace.

The other interesting thing they want to bring a style of building that you don't normally see around Halifax but you would see in other cities, it's called a thin-point tower. Mr. Haggett mentioned that quite often you would see 110' building square on a lot like this, however his client wants to build a tall thin elegant building for two reasons - livability and design. A thin tower creates less shadow and moves quicker over the landscape (has less of an impact) and also helps achieve and maintain views that presently exist. While reviewing a slide, Mr. Haggett reviewed the elevation showing Carlton Street in the middle and then on Spring Garden Road walking down to the Public Gardens, walking along where the addition would be, it would be refaced and update Spring Garden Terrace. The key aspect being considered was what the appropriate height would be. They looked at taller buildings but then started to hear from the public regarding Carlton corridor study and also other area design principles. There is a nice envelope where this building is within the kind that visually complements the character of the exiting community. Mr. Haggett displayed a rendering showing how the proposed building tucks nicely away from Spring Garden Road and explained the reason was because they wanted the pedestrian experience for the thousands of people who walk up and down Spring Garden Road every single day to be maintained. Mr. Haggett also displayed a rendering of the proposed new commercial addition on Spring Garden Road and explained that the intent for this would be to help continue the retail experience like across the street.

Mr. Haggett explained that along Carlton Street there is a pool area that will be part of the renovation and the parking entrance will be maintained where it presently is. There will be a garage door for privacy and protection and it will also give access to the new parking area that will be for Carleton Terrace and it will be joined to the existing Spring Garden Terrace parking. There will be enclosed underground parking with 218 proposed stalls. There will be a 2 storey street wall which will help maintain the pedestrian experience because that is where the public's visual focus and the building will be glazed by 75%. There will be a high percentage of glazing and reflective materials which will help make the building visually lighter and more elegant. Across from the cemetery will be maintained and will be landscaped nicely with the terrace

continuing out. Mr. Haggett displayed a rendering of the step down area on Garden Crest explaining that, as understood and heard from the public in March 2010, the steps down will be the same height as the existing fence is now so the visual impact will be reduced. Further the court yard area will be all landscaped so residents will no longer be looking on to a parking lot. Along Garden Crest and Summer Crest is where the terrace presently ends and that extension will be where parking is located underneath and it will be all landscaped.

One resident asked how many feet above the 20 storeys is the new building because the rendering is showing 20 storeys tall but it looks about 25 storeys and it seems high. Mr. Haggett advised he would have to figure that out but the total height of the building is 210'.

While reviewing floor plans of the proposed building, Mr. Haggett explained that there will be 6 units per floor except for the penthouse levels where there will only be 4 and that each of the floors (except the penthouse) will accommodate two 1-bedroom units and four 2-bedroom units. The total square footage will be 6,800 sqft and most typical floors are about 10,000 sqft in the Halifax region.

Mr. Haggett and his client, Killam Properties, explained they understand views are important to residents, both in the Spring Garden Terrace and adjacent properties and wanted to point out because of the measurements of the proposed building, the north view continues to see the MacDonald Bridge, so this view will be maintained. With the exception of a few areas there are many windows on Spring Garden Terrace that unfortunately their 3rd bedroom windows along the back units will be impacted by the proposed new building. There will still be some adjacent views but it will be severely impacted, however both the west and east side views from those units will be maintained. Mr. Haggett displayed renderings of the views from the East when Carlton, Spring Garden and Summer Crest were built as they interrupted the view somewhat but they did a very sensible design which had this courtyard area so the majority units on Spring Garden Terrace still enjoyed that view to the East and are expecting that view to be maintained. The western view of Dalhousie University will also be maintained.

With the placement of this building in the northwest corner, shadows have a reduced impact on the majority of the buildings and this was demonstrated on various slides showing views from the north, south, west and east of the building showing what will be affected in the views due to shadowing. Mr. Haggett also advised that additional shadows will be added in the morning on Spring Garden Road with very little shadow hitting public gardens. He continued to explain that as the sun rotates through, sunlight starts to merge on the properties continuing through to 3:00 pm, and continuing around from 4:00 pm during equinox until approximately 6:30 pm shadows will hit Garden Crest which will create a very thin impact on the Public Gardens. Part of policy requires Killam to consider minimizing any impact on the Public Gardens. Mr. Haggett advised that broader buildings actually create a much larger shadow than taller, thinner buildings. During summer solstice when the Public Gardens are very busy shadows will hit around 3:00 pm and continue on with very little shadows hitting the Public Gardens compared to the already existing shadows.

One resident requested Mr. Haggett review the shadows portion of his presentation one more time for clarification and Mr. Haggett obliged.

Community Aspects

Mr. Haggett explained that building and enabling people to meet one another and having a chat and opportunity to develop occasional friendships are very important to urban life and is one of the reasons we live downtown. Upon entering the building, access to Spring Garden Terrace will be

established on the first floor so the new amenities will be able to be enjoyed by all tenants of Carlton Terrace and Spring Garden Terrace. There will be a fitness centre, gathering space for the community to enjoy, a nice theatre, a renovated pool area and a proposed boardroom and common room.

Mr. Haggett advised that Killam always, in their new properties, have a high standard of excellence for their public amenities. Their fitness centres are accommodated with professional grade equipment and men's/women's locker rooms with private showers.

Outdoor amenities can also be used year round as there will be almost $\frac{3}{4}$ of an acre of landscaping which encompasses almost $\frac{1}{2}$ the total properties. The terrace will be expanded along Camp Hill Cemetery with nice walkways and seating areas. The pool will be upgraded during construction and new landscaping that will be enjoyed by residence and also passers by.

Mr. Haggett explained that the public realm will be key as not only do we have an opportunity to contribute to the community of Spring Garden Terrace and adjacent properties but for all the thousands of people that walk by. Killam will look at doing additional street trees and working with the City and negotiating with them on this along both Spring Garden and Carlton Terrace. Mr. Haggett proposed the possibility of a new turn-around to make it easier for people to negotiate into the road and also there will be very modern chain-link gates at the end. Camp Hill Cemetery is a beautiful cemetery and Killam would like to work with the City and potentially propose doing a more historic gate treatment there that will intrigue people to come and visit the historic gravesites.

Mr. Haggett advised that environmentally Killam takes the lead and there are 10 aspects they have in their buildings regularly. They are always looking for leading edge technology to reduce water and energy consumption in the overall environment. The environment is key to help people to walk and have other options besides vehicle choice. This is supported in the Regional Plan and is something Killam would like to see in Halifax. Mr. Haggett continued to advise that based on the Regional Plan this area has an incredible walk score and an opportunity with the new cross country connector, which is to be finished in the next year about 6 blocks away, will give people an alternative to vehicle use. The proposed site is close to education centres, health centres and the business district encompassing some of the best shopping in the city.

Mr. Haggett explained that economically the proposed project will renew Spring Garden Terrace and highlight the aspects he reviewed in his presentation and will be a way to update and refresh the building while also offering the economic opportunity of bringing more people downtown to increase the customer base to support local businesses. In the last 5 years there have been many young entrepreneurs under the age of 30 starting some very interesting businesses in our City and Killam would like to see those continued and supported with this type of project. Economically, as encouraged by the Regional Plan, it will be an efficient use of HRM infrastructure in the capital district; renewal of Spring Garden Terrace exterior will be an estimated \$30 million dollars worth of investment to build a building, job creation and long term increased tax base. Killam's intent will be to continue to contribute to the sustainability of the core. Carlton Terrace will offer improved access from friends and amenities, a cleaner environment, promotion of physical activity and many economic benefits.

Mr. Haggett advised that the construction timeline for this project will be approximately 18-24 months before construction will begin. Killam will ensure to work with the residents of Spring Garden Terrace and accommodate any needs as the pending construction time would be approaching. Killam is more than willing to listen and create some accommodation ways of

working with any issues that may come up.

Mr. Haggett explained that parking will be maintained and there will be a temporary access for the underground parking. He reassured residents that regarding the excavation noise, Killam will be following and respecting the by-laws and the NS regulations with regards to noise and excavation. Mr. Haggett advised that the building will have a baseline done and there will be a tour of the building in regards to plaster. Prior to any blasting occurring there will be a walk-thru the entire building and if any loose plaster is noticed, that would be fixed before any blasting occurred.

Mr. Haggett advised that after this meeting tonight Killam Properties are more than willing to hear from the residents with any concerns or comments.

4. Questions and Comments

Fred Richardson, Garden Crest, asked what the proposed time or final completion of the project will be? Ms. MacLellan explained that a development agreement generally takes 7 – 9 months however that time frame can vary. Mr. Richardson inquired as to the period of construction. Mr. Haggett, Genivar, advised it will be approximately 18-24 months from this date before construction will begin, and after that they anticipate approximately 2 years. Therefore the opening of the proposed building will be approximately 48 months (4 years) from this meeting date.

Pat Bowes, Emerson Towers, questioned where the 20 floors stop and the roof part continued making the builder taller. She wanted to know if that was correct or an illusion of the drawing? Mr. Haggett, Genivar, confirmed that the total height of the building is approximately 210'. Ms. Bowes asked what the height will be from the 20th floor to the top of that 210'. She also inquired as to why not 16 floors and be uniform; why go the extra height/floors? Ms. Bowes is gravely concerned about losing the view out her kitchen window which will be blocked with the proposed building. Mr. Haggett explained that it was an architectural decision and originally it was going to be higher but was brought down to 20 storeys. Ms. Bowes stated her concern about helming everything on Carlton Street, and having additional bodies come out Carlton Street, that it will not handle it no matter how many pretty gates there are. She continued to state that they are going to lose Carlton Street in the excavation with the machines going down Carlton Street and if they cannot get in there to dig at a particular time, where are they going to put them; they will be put them on Carlton Street. She stated that it will be a mess and Spring Garden Road is already a mess so there is a grave concern over this to everyone. Ms. MacLellan advised there has been a Traffic Impact Study (TIS) completed and prepared by the applicant and is currently under review by HRM traffic staff. She also advised that the new 2012 TIS should be on the website with this application and if not, she will make sure it is there, and what is on the website for the application is the most current HRM has on file. Ms. Bowes asked if a new traffic study will be done or will the 2003-2009 one be used? Ms. MacLellan advised that she will ensure the information on the website is the most current information available.

Carolyn Scott, Summer Street, asked if any wind impact studies have been performed on how that building will affect Carlton Street because currently the wind on the Tupper entrance is horrendous with the north wind. She mentioned that most big buildings add wind impact studies and that she will talk to the City to advise what they should do.

Ms. MacLellan advised that the applicant had submitted a preliminary wind impact study and HRM has asked for a more detailed study to assess the wind impact in this area.

Ms. MacLellan advised regarding the TIS question, the current version is from 2012 and she will ensure it gets uploaded on to the HRM website.

Dennis Ryan, Garden Crest, commented that this is going to be very intrusive as the proposed building is right behind him and he is at the top. He noticed during the presentation that at 3:00 pm, most days, he is going to lose the sun and that the building is a weird sort of dimension and it looks a lot smaller than what it is, that it is twice as high, and the architect presented it very well. Mr. Ryan advised it looks like it is a done deal and asked if it was, and does it have to be 20 storeys; can it not be 16 or 10 stories like everything else? He also inquired as to how formal complaints can be relayed, via in letter form or a lawyer? Ms. MacLellan explained that the 20 storeys is not a done deal and this is just an application and the PIM is used to receive feedback on the application. She advised that can be contacted via phone, email, or letter on the proposed development.

Ted MacLean asked if anything has been approved by-right as of now. Mr. MacLean also asked if Killam has the right to building 10 storeys today or so many units today or none at all. Ms. MacLellan advised that currently there is a DA on the property therefore that DA will need to be amended if there were any DA changes to the development. Ms. MacLellan continued to explain that right now, as the Land Use By-law (LUB) stands, construction can go to 45' however it is capped at a density of 250 persons per acre so HRM would have to see what the density of the property is now. Mr. MacLean was looking for clarification that 45' is 3-4 storeys and inquired as to whether the proposed lot is a separate lot or was it all part of one PID originally when it was purchased. Ms. MacLellan explained that she cannot speak to when it was purchased or whether it was a single property or two properties. She advised that it has been a single property for years and that when HRM researched the property on the Secondary Plan, which was performed in the 1980's, it was one property. Mr. MacLean asked what the expected tax revenue would be from the property at 20 storeys. Ms. MacLellan explained that she could not comment on taxes as they are based on assessment from the Province. Mr. MacLean expressed that it would be important for residents to have an idea what additional revenue will be created by such a building. Ms. MacLellan explained there are so many different variables so HRM cannot comment at this time. Mr. MacLean stated that he thought HRM should start looking at that because it is fairly important as a resident that lives in the area there and there are many other concerns I have but the additional tax revenue out ways less parking for me, more people. Mr. MacLean continued to express there are 2 obvious windfalls; there is all that is allowed to be built today is 4 storeys and if the development is allowed to go to 10 or 20 storeys, then there is a significant financial windfall for the development by the additional 70-80-100 units, and then as a taxpayer it would financially make sense for HRM to come up with a position as to how this is going to help the citizens of the area and not so HRM can collect tax revenue and then have it subsidize outside of the city when it should stay to develop the city core.

Ted Wellington, Spring Garden Terrace, explained that he is a current resident and noted the proposed building is going to be built in his backyard and he is in favor of the project. Mr. Wellington asked about the affordability because if you go higher, it will affect the prices more favorably at 20 storeys and if it is going to inflate than how much more expensive will it cost to live here? Mr. Wellington commented on increasing the density in the city and sees this as a fine building and something that HRM needs.

Ed Spartan, expressed that he lives in a Killam apartment building and had intended to live the rest of his years there, however there is a question that is considerably important to all the residents and that is that building a very nice new apartment building that has many amenities, but you're reducing the space. He continued to express that the space crowded now and a normal

reasonable space that already exists is being taken away and feels that it would greatly reduce the convenience for the present residents. Mr. Spartan also expressed concern about the effect on rents because he does not feel the new building is for the lower class, that it is probably an upper class building. He was wondering if after the proposed new building is built, does Killam know how it will affect the rents that are presently in affect in the original building because they are currently increased by 3% every year whether they need to or not. Mr. Haggett advised that Dan Sampson was at the meeting from Killam Properties and that he could answer that question. Dan Sampson, Killam Properties, explained that with the added new amenity spaces there may be a slight increase in rent. Mr. Spartan asked if it would be over the 3%. Mr. Sampson said yes it could be but that is 3 – 4 years from now. He further explained that if a unit is not being renovated in the older building then there should not be any significant increases as it is mainly only in the units that will be renovated. Mr. Spartan expressed that 3% is a significant increase every year and he, as are many other residents, are on a fixed income so any increase in the present building due to the new building is a disadvantage. Mr. Sampson advised that the 3% increase does not begin to cover the increase in water bills, electricity, and taxes so Killam feels 3% is reasonable. Mr. Spartan advised that the electricity doesn't come out of Killam; the residents pay their own electricity. Mr. Sampson said that Killam pays for the common areas, but they don't anticipate huge increases. Mr. Spartan said they are getting the general picture that the rent will go up even more than they are now. Mr. Spartan asked for confirmation regarding the blasting and that Killam is only going to go one depth into the ground for underground parking? Mr. Haggett confirmed the depth will be 20'. Mr. Spartan asked for an estimated time frame as to how long it will take to excavate that area and how many months of blasting will occur? Mr. Haggett advised it will take approximately 8 months for excavation. Mr. Spartan expressed concern about the noise as it will be quite disturbing and with old age not sure if it's healthy or not. He did not think this is a good idea and that Killam should be very careful and improve its present building and find another spot that is more reasonable for the size of the proposed building.

Dave Ring, Garden Crest, expressed that he understands the current height restriction is approximately 45' and is surprised there is an application being requested for a DA that is effectively four times that height. While respecting the fact that there has got to get pay back, but when the height restriction is 45', he is wondering why HRM would even consider that as a starting point. Mr. Ring continues to say that their understanding is that the proposed building is a rental building and one of the concerns that a number of people in his building have is the noise that is going to come as a result of these additional units. He noted that it is probably going to raise the rent and Killam has no idea what the rents are going to be, but is this going to be a building that will allow someone to rent their unit to 3-6 university students all cohabitating in the same 1100 sqft unit in this building? Mr. Ring would like to know if this is something that has been discussed or will HRM look at that? Addressing the question regarding why HRM is considering an application for 20 storeys, Ms. MacLellan explained there are policies that allow HRM to be able to consider development that exceeds the 45' height restriction with no limit on a proposed height one can apply for. She also commented on the question regarding the issue with the amount of people per unit and advised that HRM can look at doing things such as limiting the amount of bedrooms per unit but cannot control how many people live in the unit. Mr. Ring commented that he heard 250 people per acre but is not sure how many are currently there, however 250 on paper in rental units, versus what is real, depending on the rental amount, thinks there is a good argument to be made that it may be more than what some people expect. He continued to advise that a number of the residents in Garden Crest are concerned of the servicing and the restaurant that is being proposed on Spring Garden Road. Mr. Ring explained that the parking the residents currently have for guest parking right now is accessed right off Spring Garden Road and it is a challenge right now as there are concerns with garbage pickup,

etc., and the concern is that if there is a restaurant there and it goes right to the property line then the concern is access to parking for their guests to stay.

Bill Cocasham, Summer Gardens, commented that he does not think it would matter if everybody in the room carried a plaque down to City Hall; the height of the proposed building is probably what they are looking at. He said he has been to these kinds of meetings before and thinks once HRM gets this far along, the residents are not going to change it. He proceeded to express that there is a problem with the number of parking spaces that have been allowed as parking around there is pretty bad. Mr. Cocasham continued to voice the residents' displeasure with the height by mentioning again that the drawings were done in a way to make the building look smaller than it really is and that it is going to be 8 storeys higher than the existing building and that is when the impact can really be seen. He confirmed with Ms. MacLellan that she can be contacted if anyone wants to voice their displeasure with the proposal.

Penelope Russell, Summer Gardens, asked if the glassed-in area, as shown in the drawings of the new building, are penthouses, public space or is it covering the heating and air conditioning units? She also inquired whether the amenities in the new building are going to be available to all the residents of Spring Garden Terrace or just for the residents of Carlton Terrace. Ms. Russell concluded by stating that she is assuming there will be balconies on the south and north sides only. Mr. Haggett advised that the glassed-in areas will be penthouses and levels 19 and 20 will house 4 penthouse units per floor along with wrap-around terraces for the penthouses, so there will be 4 terraces, one on each corner. He continued to advise that the new amenities will be for all residents of both Spring Garden Terrace and Carlton Terrace and there will be no additional charge to utilize these amenities as it will be included in the rents.

Susan McCurdy, Victoria Road, expressed how disappointed she was in the image presented at this meeting as it is quite misleading about the height of the building and that is really poor form and at the next meeting something should be made more realistic. She continued to say that she lives in an 1860 home in Halifax and has invested tens of thousands of dollars to restore it and a while back there was blasting that took place around South Park Street and Fenwick Street, and the walls of all the beautiful old homes were shattered. Ms. McCurdy explained that the blasters were called in to take a look and brought in their machines and wrote a form letter that had nothing to do with them, therefore she wanted to warn residents in the community not to expect anyone to come and repair their walls/house and their properties will vibrate, scaring you frequently. Mr. Haggett commented that with regards to the blasting, there will be a baseline for all surrounding homes and that all the rules and regulations surrounding that will be followed and adhered to. Ms. McCurdy insisted that she was told the same information and nothing was done regarding the cracks that happened at her house because of the blasting.

Pat Bowes, Embassy, expressed her disappointment in the height of the proposed building. Ms. Bowes advised that condo owners pay higher taxes than rental apartments. She continued by stating that residents bought these condos, some as their last home, have spent tens of thousands of dollars on them, or the view, or the quiet, and now they will be losing that while gaining more congested traffic, while still paying higher taxes with no views. She expressed concern about not getting the value of their homes back when trying to sell their condos in the future after this development is finished. Ms. Bowes said she agrees that these building should be created, however 20 storeys is excessive and does not believe it is fitting in with the pattern because their building is currently 16 storeys and if the new building was 16 storeys or less, then it would fit within the pattern. She does not believe the tall, triangle, configuration is needed. Ms. Bowes continued to express the concern in their building with the proposed blasting because millions of dollars have just been spent on the building to replace all the brick and millions more dollars are

planned to be spent fixing it up before you even break ground. She strongly stated that she is not going to go back on “oh we’ll take care of it” because that could just be blowing in the wind and “you can plant your flowers in it”. A lot of money has already been spent and she believes Halifax should have growth, but thinks there has to be some accountability and does not feel HRM is accountable and that they do not care.

Loraine, Spring Garden Terrace, asked where the guest parking will be located because currently there are parking spaces against the fence that is available to rent for guests and they currently rent one of those spaces. She feels that is all going to go so is very interested to know if Killam will be allowing for guest parking somewhere. Ms. MacLellan advised that HRM staff will be looking into that requirement in the DA to ensure there will be parking devoted to guest/tenant parking or commercial parking. Mr. Haggett also advised that there will be some guest parking

Tonya Russell, Embassy Towers, feels there will be many problems with putting a 20 storey building there, such as not providing enough parking for residents and visitors, and placing the entrance on a small dead end street which is already a problem for traffic coming, service vehicles arriving, etc. Ms. Russell agrees that city center should be densely populated, but what HRM needs are buildings for those requiring assisted living and nursing homes, not more high standard expensive homes for young professional people who can drive from the city.

Alan Purdy, Garden Crest, agrees that height is the main issue and felt it should be addressed. He questioned why the representative of HRM seems to be facilitating this process because, he felt, Ms. MacLellan seemed in support of the project rather than finding a balance useful for the best of the community. He felt that he was hearing a dual conversation in support and did not hear Ms. MacLellan, or anyone else from HRM, say anything that might be in support of what the residents want. Mr. Purdy commented that there is a huge difference between owning a home/condominium and renting one and stated that he totally disagreed with the production and erection of the proposed building. He continued to state that he could not understand HRM breaking its own general rule of 45’, and that HRM can do special circumstances reminds him of building the Trade Centre downtown when the city and the government bent all their rules and allowed it go ahead without due process. Mr. Purdy stated “for the record” that he did not think Ms. MacLellan was representing a balanced point of view from the point of view of HRM and that she was working with Killam and Genivar. He expressed that he felt Ms. MacLellan should be doing more to help the citizens of Halifax and the people that live in these areas because Killam Properties have the opportunity to build anywhere they want as they obviously have the money, otherwise they would not be doing this. Mr. Purdy felt that Ms. MacLellan should not facilitate the process because he felt she had a bias point of view because of the way she presented by defending all the statements and that she was not doing anything with the people’s requests at the meeting but saying “we’ll look into that”, and that was not what he was hearing, and that was his opinion. Ms. MacLellan clarified why she was facilitating the meeting by stating that the purpose of a PIM is to get feedback from the public so that staff can go back and look further into what the comments are and see whether or not it is appropriate based on the comments received from the public. Mr. Purdy advised that he respected that and understands the process but felt she was advancing on the process more towards the developer than the citizens and wanted to hear her say “what are the negative aspects of this as well?” Ms. MacLellan explained that she personally cannot have any comment on the application because it is still under review and that staff is trying to ensure, via this meeting, that the public concerns are heard and staff will be reviewing other aspects of the application as well. Mr. Purdy explained that he understood Ms. MacLellan will be representing them and others to HRM staff and just wanted “the record to indicate” that he did not feel Ms. MacLellan was acting in their best interest.

Rosemary Marr, Embassy Towers, explained that in 1970 she was involved in planning for the south end and coordinated with the residents and City Planning and was told that if apartment or office buildings were built, that they will not be permitted to go higher than four storeys. She continued to say that taxes keep going up because the values of condos are increasing, yet Killam Properties say their rents are going up because their taxes are going up but we attended a meeting a while back and were told that apartment rentals go down in value and taxes because of the depreciation. She expressed her confusion about why a rental goes down because of depreciation but a condo goes up and that is something that is perplexing a lot of condo owners. Ms. Marr feels the building is a nice looking building but too high and wondered if before digging starts will all the condos be inspected in Embassy Towers for cracks because she has already paid to have cracks filled in her unit from when the bricks were removed from the building and she does not want to have to pay to have this done again. Ms. MacLellan advised she will look further into what the requirements are for the blasting and I will check with HRM engineers to see what is required for a pre-blasting survey.

Roberto Antonio, Embassy Towers, asked if residents do not accept the height, can that be changed. Ms. MacLellan explained that staff is at the early stages of reviewing for the height in the application. Mr. Antonio asked what the possibility would be for only 1 – 10 storeys. Ms. MacLellan advised she could not comment. Mr. Antonio asked why 20 storeys was selected and not 40, and do the residents have to accept that? Mr. Haggett advised that the original conception 2.5 years ago was to build a 32 storey tower and then it was revised to 26 because there were design principals we were considering and the architect's vision. He continued to explain that as the developer started to understand what communities comfort are at, the application was revised to 20 storeys and the architect again made that narrow point tower. Mr. Haggett explained that the goal with the narrow point tower was to reduce the visual impact of a taller building at the street, both on views as well as shadow and as well as pedestrian experience. These are part of the consideration, not just the height but so is the visual impact to both adjacent properties as well as to the passer byer on Spring Garden Road and the design aspects as shown on the slides, is the design consideration for the proposed 20 storey tower. Mr. Antonio asked for clarification on what is meant by visual impact because he lives on the 12th floor and I will still see 8 more floors on top therefore the visual impact will be dramatic. Mr. Haggett stated he would be available to speak with any resident after the meeting. He also stated that Killam was trying to reduce the impact by making a taller, thinner building. Mr. Roberto asked about the rebuilding of the balconies on Embassy Towers and wanted to know if they were going to be maintained as it is now with rusty balconies, etc. or will it be improved? Mr. Haggett explained that the building is presently maintained, and continues to be maintained on a regular basis since Killam purchased the building and that the intent, through this new building being developed, will enable a complete updating of the exterior of Embassy Towers and deteriorating faces and those rusty balconies will be updated and maintained.

Peter Rouvalis, Quinpool Road, said that everyone has been commenting on the fact that the sketch shown does not show a true impression of what kind of impact it will have on the area which he feels 8 storeys is significant but if another view was provided, and if we do another PIM, another view to see would be of the corner of Carlton and Spring Garden Road where there are two heritage properties and the view to Embassy Towers where it was a 16 storey addition. He believes that things need to be built because the city needs density and hopes that when he ages there will be something where I can move into and have a view. Mr. Rouvalis commented that if people can move past what have always been held on to and he understood everybody at the meeting and their concerns because everybody is getting older, but the reality is if the city is to move forward, we need to have change. He felt the biggest concern was about the money and

his biggest concern, living on the peninsula, is \$130 million dollars leaving the peninsula to service the suburbs.

David Reign, Garden Crest, asked if there will be copies of the presentation available on-line. Ms. MacLellan confirmed that a copy of her presentation will be on the website tomorrow, but she will have to inquire with the applicant to possibly but theirs on as well. Mr. Reign asked what the estimated time when a Public Hearing (PH) may happen? Ms. MacLellan advised probably not until next year (2014).

One resident asked if the minutes will be put on the website. Ms. MacLellan advised that the minutes will be part of the staff report when it goes to Council so that will be online when the staff report is available.

David Reign, Garden Crest, asked if the minutes will have the answers to some of the questions that have been raised at this PIM, or just what was said? Mr. MacLellan confirmed the minutes will just have the discussion that happened during the PIM, however if anyone would like to have answers to some of the questions asked tonight, she could speak with them afterwards to ensure their contact information was obtained so she could provide answers directly to them.

Michael McCurdy, Victoria Road, asked if there was any indication as to proposed rents that will be charged in proposed building, for example a two bedroom unit? Mr. Haggett explained that due to the fact that we are four years out before opening, and there are so many variables, it would be impossible to say at this time. Mr. McCurdy asked if it was opening tomorrow what would be charged for rent. He was shocked that Killam would be going into this without some kind of idea as to what kind of profits were to be made from the construction of the proposed building and that it must have been considered as part of this proposal. Mr. Haggett advised that the new units will be approximately \$1800 and up on average, depending on the floor height. Mr. McCurdy said he feels they should be thinking more in terms of affordable housing that older people are going to be requiring in order to stay within the community they have lived most of their lives and he did not see that as part of this proposal and would like to see HRM consider that perhaps that has to be worked into the equation if HRM is going to allow development to take place on the peninsula, then perhaps the developer has to consider some of the units being allocated for people that cannot afford the housing. Mr. McCurdy referred to the Trillium building and when that building went up it was supposed to offer some affordable housing for young professionals to live on the peninsula and when the finished product occurred you had to be in the ½ million dollar and up bracket, and he felt the city should demand from developers some kind of control on that to allow people to live in this area.

One resident asked if Ms. MacLellan is who emails or letters of concern are to be sent to. Ms. MacLellan confirmed that she is the contact and if she receives emails than she can email back a response.

Carol vonSyberg, Garden Crest, asked if there will be a model of the building. Ms. MacLellan advised that the applicant provided a sketch of the model and will check on having it put on the website.

Thomas Barkton asked who can be contacted with questions or concerns regarding this project. Ms. MacLellan advised that her business cards were on the table out front which can be picked up after the meeting. Mr. Haggett also advised that he is available to residents at any time.

5. Closing Comments

Ms. MacLellan thanked everyone for coming and expressing their comments and concerns.

6. Adjournment

The meeting adjourned at approximately 9:00 pm.